

CITY OF BALTIMORE

BERNARD C. "JACK" YOUNG, Mayor



DEPARTMENT OF TRANSPORTATION

Steve Sharkey, Director
417 E. Fayette Street, 5th Floor
Baltimore, Maryland 21202

ADDENDUM NO. 1

DATE: February 14, 2020

FOR
DRAWINGS, SPECIFICATIONS, PROPOSAL, CONTRACT AND BOND

FOR
CITY OF BALTIMORE CONTRACT NO. TR-17302
F.A.P. No. AC-STBG-3241(5) E,
S.H.A. No. BC410024,

S. CLINTON STREET REHABILITATION FROM BOSTON STREET TO KEITH AVENUE

FOR THE MAYOR AND CITY COUNCIL OF BALTIMORE

BID DUE DATE: March 4, 2020

TO THE BIDDERS: PLEASE ATTACH TO YOUR CONTRACT DOCUMENTS.
THIS ADDENDUM IS HEREBY MADE A PART OF THE CONTRACT DOCUMENTS ON
WHICH THE CONTRACT WILL BE BASED, AND IS ISSUED TO MODIFY, EXPLAIN
AND/OR CORRECT THE ORIGINAL DRAWINGS AND SPECIFICATIONS.

THIS ADDENDUM NO. 1 PACKAGE IS BEING ISSUED TO REVISE CONTRACT BOOK AND
RESPOND TO QUESTIONS SUBMITTED BY CONTRACTORS.

**REVISIONS TO CONTRACT BOOK: PLEASE DELETE PAGES 94 TO 97, 202, 287, 291 TO
336, 346 TO 358, 384, 391, AND 398 AND REPLACE WITH PAGES 94R TO 97R, 202R, 287R,
291R TO 336R, 346R TO 358R, 384R, 391R AND 398R.**

APPROVED:

for  *2/18/20*

CHIEF, TRANSPORTATION ENGINEERING & CONSTRUCTION DIVISION

APPROVED:



DIRECTOR, DEPARTMENT OF TRANSPORTATION

F.A.P. NO. AC-STBG-3241(5)E
SHA NO. BC410024
BALTIMORE CITY NO. TR17302
S. CLINTON STREET REHABILITATION
FROM BOSTON STREET TO KEITH AVENUE
ADDENDUM NO. 1

The following questions were received and are answered here for clarification:

1. I noticed that there are full size drawings given for the erosion and sediment control section. Would it be possible to provide full size drawings for the remainder of the project? The current drawings given at the back of the spec book are not to scale and difficult to decipher for a project of this size and complexity.

***Response to Question #1:** 8.5x11 scaled drawings are included as part of Addendum No. 1.*

2. Page 33, **Contractor and Railroad Public Liability and Property Damage Insurance**, in order to obtain coverage the contractor will need specific information; such as, daily counts for train traffic on all track crossings, types of trains. Can the City provide this information?

***Response to Question #2:** The Contractor must have railroad insurance; refer to page 33 and SC-33 page 79. Please bid accordingly.*

3. Page 71 and 72, **C-14 General Electrical Work and Testing**, will there be any electrical work under this contract? There are no electrical pay items in the contract. Also, does the traffic light at Boston Street and Clinton Street operated by traffic loops or cameras? If traffic loops, will they be replaced?

***Response to Question #3:** There is no anticipated electrical work under this contract. The signals are operated by cameras.*

4. Page 205, **Measurement and Payment**, states that adjustments of manhole frames and covers, inlet heads, frames and grates will not be paid, as their adjustment is incidental to the full-depth concrete paving. If so, what are Items 306, 307, 308, 507, 508 and 509 being used for? Page 207, **Payment**, states that frame and covers will be paid per the contract unit

***Response to Question #4:** Page 205 dictates that adjusting of frames and covers are incidental to the pertinent pavement item. The Contract indicates that frames, covers, and portions of existing inlets are to be replaced with pay items 306-308 and 507-509.*

5. Pages 227 through 235, all of the MOT Standards refer to flagging operations. Does the City expect the Contractor to maintain a flagging operation while concrete is curing before it can be fully opened to traffic?

***Response to Question #5:** Flagging will be provided while the work is being performed. While the concrete is curing, the repaired area shall be protected with channelizing devices.*

6. Page 339, on the right bottom description box, it states “**Northern Section Phase 1 Stage 2**”. Where’s Stage 1?

***Response to Question #6:** Refer to page 338, Phase 1, Stage 1, note 3: “The Contractor shall provide MOT plans for bump-out removal and temporary pavement placement to be approved by the Engineer.”*

7. Not to beat a dead horse with this question, but please clarify in an addendum why this project does not have full size drawings that are to scale? The erosion control drawings are full size and to scale, but the remaining project drawings are 8.5" x 11" and not to scale in the back of the spec. book. It was mentioned at the pre-bid meeting that this is a "book job." Please clarify what the definition of a "book job" is. This is advertised as a multimillion-dollar project over the course of approximately 15 months. The small size drawings are very difficult to properly read and decipher the proposed scope of work for this project. From a contractor's perspective, when there are insufficient drawings that make it virtually impossible to do our due diligence; this increases project risks and unknowns. In turn, this will increase our price for this work. Please clarify in an upcoming addendum why the engineering firms/Baltimore city cannot provide proper full size drawings for this work.

Response to Question #7: *8.5x11 scaled drawings are included as part of Addendum No. 1.*

8. Is excavation to subgrade incidental to concrete paving items 510,511,& 512?
Item 201 Class 1 Excavation: What is this item used for? How were the quantities calculated?

Response to Question #8: *Excavation is not incidental to items 510, 511 and 512. Class I excavation is calculated based on the full depth replacement of the concrete pavement, concrete driveways, installation/removal of temporary pavement. Excavation quantities have been updated as part of Addendum 1.*

9. Our insurance carrier is asking for answers to the following questions in order to provide a price for railroad protective insurance.
- Number of railroad employees at the site
 - Is hold harmless required?
 - Number and speed of passenger freight and switching trains passing in each direction
Each 24 Hr period:
Passenger # Freight# Switching#
Speed MPH
During work Hrs:
Passenger# Freight# Switching#
Speed ____ _____ _____
 - # of tracks: Mainline _____ Other _____

Response to Question #9: *The Contractor must have railroad insurance; refer to page 33 and SC-33 page 79. Please bid accordingly.*

10. Clarify when class 1 excavation will be paid. The only time it shows on the drawings is for the removal of the 10" concrete roadway. Will the excavation for the type I and type II repairs be paid under this item as well? Sheet PS-02 shows pavement removal that is "incidental to class 1 excavation." Explain everything included/not included under class 1 excavation.

Response to Question #10: *Class I excavation is calculated based on the full depth replacement of the concrete pavement, concrete driveways, installation/removal of temporary pavement. Class I excavation quantities have been updated as part of Addendum 1.*

11. Page 73, SC-15, of the spec. book addresses BGE Street Lighting work. There are no items related to electrical upgrades, clarify why this section is given in the specifications?

Response to Question #11: *This section of the Contract Book contains standard City special conditions. No BGE lighting work is anticipated for this project.*

12. Explain the contingent items. For example, there is a quantity for 3” GAB and 6” GAB with no exact locations shown, and there are contingent items for both the 3” and the 6” with much larger quantities. Is the contractor to assume that if new 3” GAB is determined to be needed by the engineer, it will be paid under item 501 for up to 370 SY, but if that quantity is exceeded, then it will be paid under item 502? There is no given reference to this, so clarify these items.

Response to Question #12: *Refer to page 288-289 of the Contract Book. The Contractor is to maintain the existing 6-inch GAB for the concrete roadway replacement and maintain the existing 3-inch CR6 for the sidewalk replacement. New 6-inch GAB is to be placed under the temporary pavement and under new curb locations. New 3-inch CR6 is to be placed under new sidewalk or sidewalk reconstruction where the existing CR6 cannot be maintained. Contingent 6-inch GAB shall be used during the concrete roadway replacement if it is determined by the engineer that the existing subgrade has deteriorated. A similar approach shall be taken for the contingent 3-inch CR6 for the sidewalk replacement. The removal of deteriorated GAB under the roadway and driveway reconstruction shall be paid per Class 1A and the removal of the deteriorated CR6 shall be paid per Class 2. Quantity adjustments for excavation have been incorporated into Addendum 1. Contingent variable depth GAB shall be used in locations where additional GAB is required and the existing GAB is salvageable as determined by the engineer.*

13. On all the PS roadway drawings under “General Pavement Repair Notes:” number 1 states “joint seals in poor condition shall be cleaned and sealed in accordance.....” and number 2 states “linear cracks 1/8” wide and wider shall be sealed in accordance...” No pay items exist for this potential work, and how is the contractor supposed to put a quantity to this at time of bid? What one contractor deems as “poor condition” might not be the same definition as another contractor, or the engineer. Add pertinent items or contingent items for this work if the city intends to perform.

Response to Question #13: *Pay items for cleaning/sealing cracks and joints within concrete pavement are included in Addendum 1. Notes will be added that joint repair will be as directed by the engineer.*

As part of Addendum No. 1, revise the Bid Book as follows:

1. Replace Pages 94-97 with Pages 94R-97R – Notice to Contractors-Request for Information
Revise: Replace Notice to Contractors special provision with the latest update.
2. Replace Page 202 with Page 202R – Category 500
Revise: Include additional items.
3. Replace Pages 287, 291-336, 346-358 with Pages 287R, 291R-336R, 346R-358R – Details and Drawings
Revise: Replace details with revised to scale details.
4. Replace Page 384 with Page 384R – Schedule of Prices
Revise: Replace schedule of prices with quantity updates.

5. Replace Page 391 with Page 391R – Schedule of Prices
Revise: Replace schedule of prices with quantity updates.

6. Replace Page 398 with Page 398R – Schedule of Prices
Revise: Replace schedule of prices with item updates.

SPECIAL PROVISIONS
NOTICE TO CONTRACTOR

NOTICE TO CONTRACTOR

NOTICE TO BIDDERS. The Proposal Form Packet in Bid Express requires the following information be submitted for the Bidder and each firm quoting or considered as subcontractors:

- (a) Name of firm.
- (b) Address of firm.
- (c) MBE, Non-MBE, DBE, or Non-DBE.
- (d) Age of firm.
- (e) Annual gross receipts per last calendar year.

AFFIRMATIVE ACTION PLAN (AAP) CONTRACT GOALS. In order to be in compliance with the revised MBE/DBE laws effective September 27, 2011 or later, the bidder is required to complete the AAP information within the MDOT MBE/DBE Form A and Form B (Parts 2 and 3) of the Proposal Form Packet for State, Federal, and State Small Business Reserve Procurements. Failure to complete the information may be grounds for the bid to be declared non-responsive.

HIGH VISIBILITY SAFETY APPAREL POLICY. The Maryland Department of Transportation's State Highway Administration (MDOT SHA) has updated the High Visibility Safety Apparel Policy which is included in this Contract. Contractor shall comply to the policy fully for the parts Contractor is responsible for.

BOOK OF STANDARDS. The Book of Standards for Highway and Incidental Structures is only available on the Administration's Internet Site at www.roads.maryland.gov. The Book of Standards can be located by clicking on Business; Business Standards and Specifications; Construction and Material Standards and Specifications; and Book of Standards for Highway and Incidental Structures.

2019 STANDARD SPECIFICATION FOR CONSTRUCTION AND MATERIALS BOOK. The 2019 Standard Specifications for Construction and Materials Book is only available on the Administration's Internet Site at www.roads.maryland.gov. The 2019 Specification Book can be located by clicking on Business; Business Standards and Specifications; Construction and Material Standards and Specifications; and Standard and Supplemental Specifications for Construction and Materials.

BRIDGE UNDERCLEARANCE. The minimum underclearances shall be maintained whenever resurfacing a roadway. This may require grinding the existing pavement prior to placing the resurfacing material. Immediately after completing the resurfacing operation and when the lane closures are still in the effect, the Contractor, in the presence of the Engineer, shall measure the minimum vertical underclearance. The Engineer will submit results to the Office of Structures. The cost of these measurements will be incidental to other pertinent items specified in the Contract Documents.

SPECIAL PROVISIONS
NOTICE TO CONTRACTOR

REQUEST FOR INFORMATION. Refer to page 74 of SC-18.

RIGHT-OF-WAY STATUS. The work on this project is to take place within property owned by the mayor and City Council and/or within the existing right of way or easements through the Baltimore City Department of Transportation. There is no relocation assistance for this project.

Refer to SC-33 on page 79 for the Railroad Coordination requirements.

RAILROAD STATEMENT. Federal Aid Contract No. AC-STBG-3241(5)E

For this project, the Administration is providing the following statement of coordination (check one):

- No Railroad coordination required (no RR facilities are affected) (check this box when there is no railroad facility within or near the terminus of the project limits)
- All Railroad work has been completed prior to the project (check this box if traffic control devices within or near the terminus of the Federal-Aid project limits comply with the current edition of the Manual on Uniform Traffic Control Devices)
- The necessary arrangements have been made for all railroad work to be undertaken and completed as required for proper coordination with physical construction schedules. (Appropriate notification shall be provided in the PS&E for railroad coordination concurrent with the project construction)
- For AREAWIDE Contracts, the Administration will provide a Statement of Coordination when the Modification to the 25C is submitted, prior to NTP. (Check this box for all AREAWIDE Projects)

Note: Refer to SC-33 on page 79 for the Railroad Coordination requirements.

SPECIAL PROVISIONS
NOTICE TO CONTRACTOR

REQUIRED ENVIRONMENTAL PERMITS, APPROVALS AND AUTHORIZATIONS.

The Administration will obtain all required permits, approvals, or authorizations which are within the project scope and limits set forth in the contract documents and listed in the below table. The Contractor shall comply with the requirements of all permits, approvals, or authorizations required for this project. All permits received by advertisement are included in the IFB. Permits received after advertisement and prior to bid opening will be added to the IFB via an addendum.

All of the indicated permits, approvals, and authorizations should be kept on-site unless indicated otherwise. Proposed changes to the project may require additional permits, approvals, and authorizations and/or modifications.

Permit/ Approval/Authorization Description	Required for this project?	Approval/ Permit/ Authorization Included in IFB? ¹	Permit, Approval, Or Auth. Number	Expiration Date
WETLANDS, WATERWAYS, CRITICAL AREA				
MDE Non-tidal Wetland & Waterway Permit	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input type="checkbox"/> Yes <input type="checkbox"/> Draft <input type="checkbox"/> No		
MDE Authorization to Proceed	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input type="checkbox"/> Yes <input type="checkbox"/> Draft <input type="checkbox"/> No		
MDE Letter of Authorization	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input type="checkbox"/> Yes <input type="checkbox"/> Draft <input type="checkbox"/> No		
MDE General Waterway Construction Permit	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input type="checkbox"/> Yes <input type="checkbox"/> Draft <input type="checkbox"/> No		
MDE Water Quality Certification	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input type="checkbox"/> Yes <input type="checkbox"/> Draft <input type="checkbox"/> No		
MDE Tidal License	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input type="checkbox"/> Yes <input type="checkbox"/> Draft <input type="checkbox"/> No		
MDE Tidal Permit	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input type="checkbox"/> Yes <input type="checkbox"/> Draft <input type="checkbox"/> No		
MDE Tidal No-License	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input type="checkbox"/> Yes <input type="checkbox"/> Draft <input type="checkbox"/> No		
Maryland State Programmatic General Permit	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input type="checkbox"/> Yes <input type="checkbox"/> Draft <input type="checkbox"/> No		
COE Individual Permit	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input type="checkbox"/> Yes <input type="checkbox"/> Draft <input type="checkbox"/> No		
U.S. Coast Guard Permit	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input type="checkbox"/> Yes <input type="checkbox"/> Draft <input type="checkbox"/> No		
Critical Area Commission Approval	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	<input type="checkbox"/> Yes <input type="checkbox"/> Draft <input type="checkbox"/> No	8/6/18	
MDE Water Appropriations Permit for Ground Water	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input type="checkbox"/> Yes <input type="checkbox"/> Draft <input type="checkbox"/> No		
Other	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input type="checkbox"/> Yes <input type="checkbox"/> Draft <input type="checkbox"/> No		
EROSION/SEDIMENT CONTROL & STORMWATER MANAGEMENT				
Stormwater Management and Erosion & Sediment Control Approval	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	<input type="checkbox"/> Yes <input type="checkbox"/> Draft <input checked="" type="checkbox"/> No	ESD7483 4/26/19	

SPECIAL PROVISIONS
NOTICE TO CONTRACTOR

Permit/ Approval/Authorization Description	Required for this project?	Approval/ Permit/ Authorization Included in IFB? ¹	Permit, Approval, Or Auth. Number	Expiration Date
NPDES Permit for Stormwater Associated with Construction Activity ²	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	<input type="checkbox"/> Yes <input type="checkbox"/> Draft <input checked="" type="checkbox"/> No	MDRCD04B6 5/6/2019	
AASCD Approval	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input type="checkbox"/> Yes <input type="checkbox"/> Draft <input type="checkbox"/> No		
TREES				
MD Roadside Tree Permit	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input type="checkbox"/> Yes <input type="checkbox"/> Draft <input type="checkbox"/> No		
Maryland Reforestation Law Approval	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input type="checkbox"/> Yes <input type="checkbox"/> Draft <input type="checkbox"/> No		
Maryland Forest Conservation Act Approval	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input type="checkbox"/> Yes <input type="checkbox"/> Draft <input type="checkbox"/> No		

¹ ‘Draft’ indicates the formal permit has not been obtained but draft permit conditions are included.

² See website listed below for complete NPDES Permit requirements:
<http://mde.maryland.gov/programs/Permits/WaterManagementPermits/Documents/2014MDRC-GeneralPermit.pdf>

Abbreviations:

AASCD - Anne Arundel Soil Conservation District
 COE – U.S. Army Corps of Engineers
 MDE – Maryland Department of the Environment
 NPDES – National Pollutant Discharge Elimination System

**CATEGORY 500
PAVING**

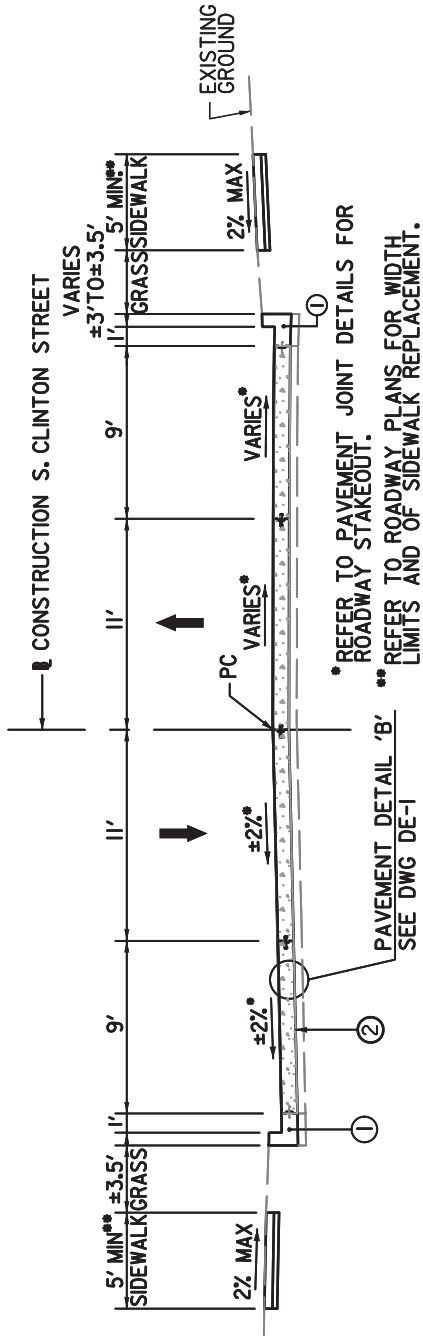
ITEM NO. AND DESCRIPTION	SECTIONS
515 CONVENTIONALLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT TYPE II REPAIRS	520, SP
516 REMOVAL OF EXISTING PERMANENT PAVEMENT MARKING LINES, ANY WIDTH	558
517 5 INCH WHITE CONTRAST PAVEMENT MARKING TAPE	551
518 5 INCH YELLOW CONTRAST PAVEMENT MARKING TAPE	551
519 24 INCH WHITE PREFORMED THERMOPLASTIC PAVEMENT MARKINGS	554
520 WHITE PREFORMED THERMOPLASTIC PAVEMENT MARKINGS LEGENDS AND SYMBOLS	554
521 CLEANING AND SEALING OF CRACKS IN EXISTING CONCRETE PAVEMENT	523
522 CLEANING AND SEALING OF EXISTING CONCRETE PAVEMENT JOINTS	523

NO.	DESCRIPTION	DATE	BY

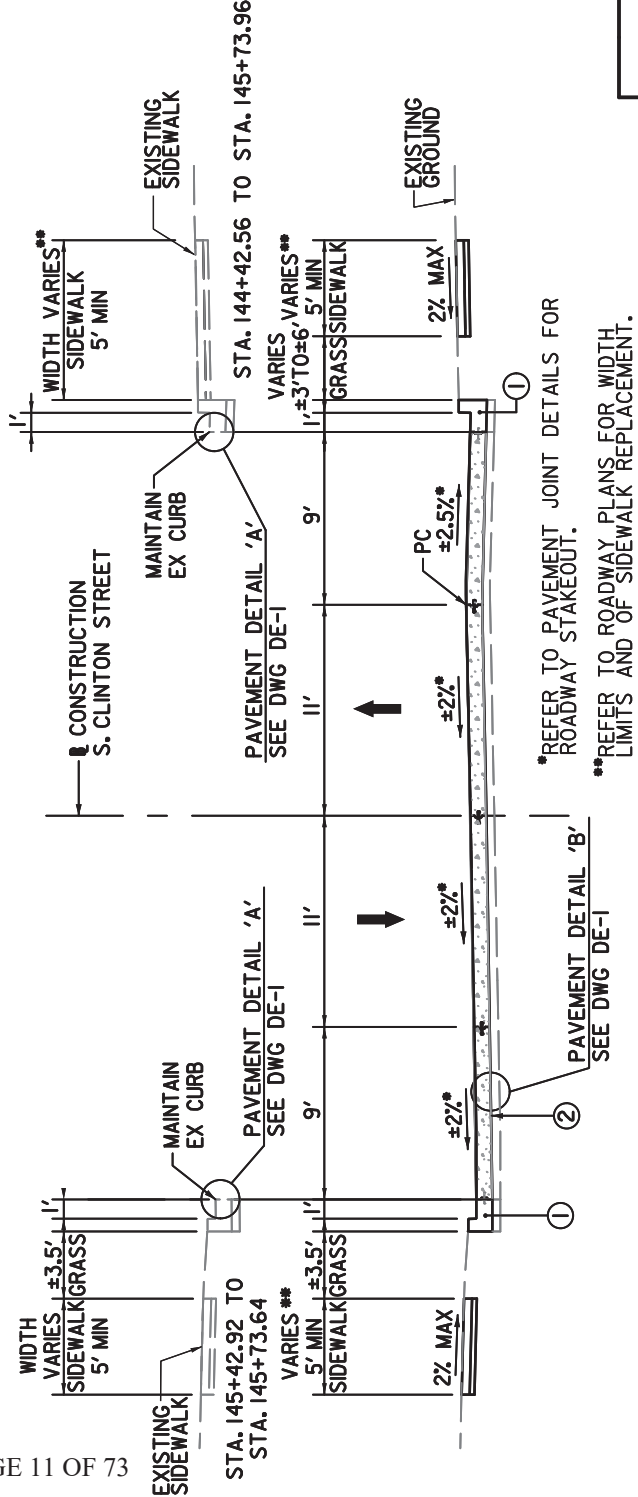
LEGEND

- ① TYPE A CURB & GUTTER
- ② LIMIT OF CLASS I EXCAVATION

NOTE: CLASS IA EXCAVATION SHALL NOT EXCEED 6 INCHES UNLESS OTHERWISE DIRECTED BY THE ENGINEER.



S. CLINTON STREET
STA. 148+50.00 TO STA. 150+25.00

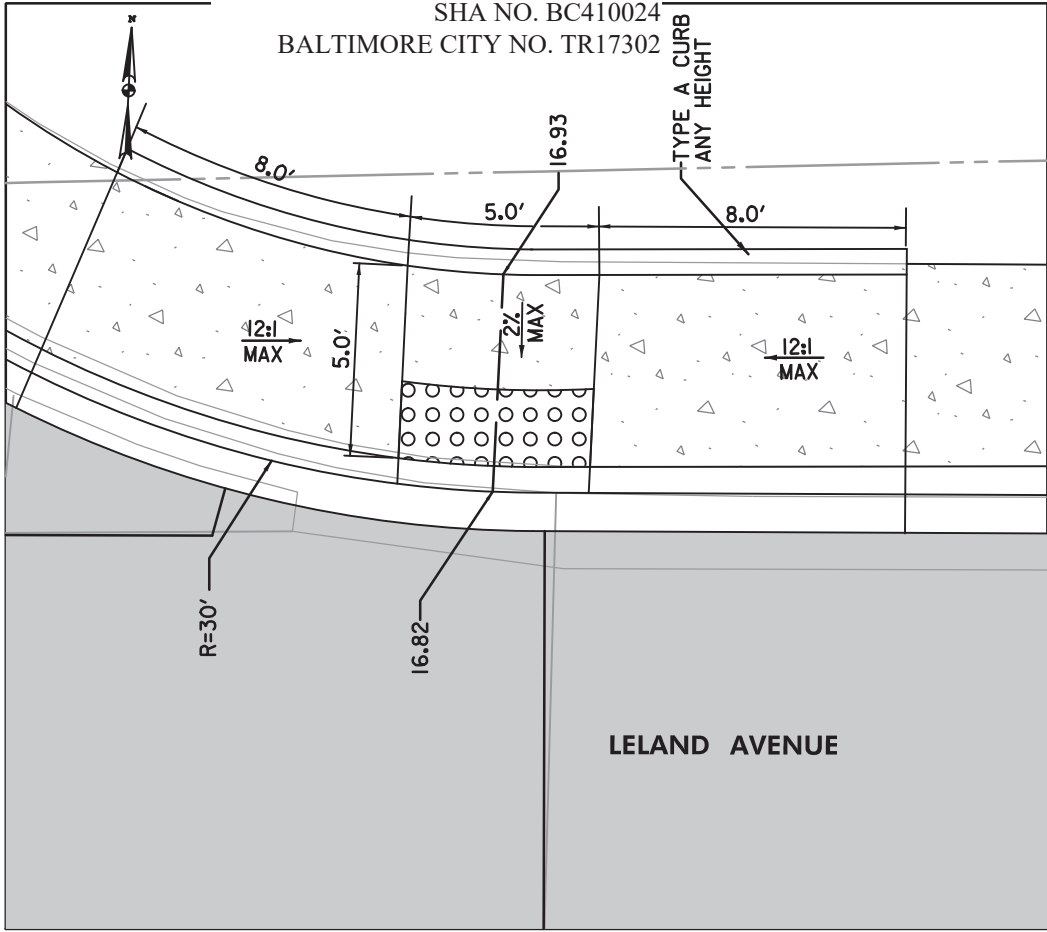


S. CLINTON STREET
STA. 137+54.00 TO STA. 148+50.00



REVISIONS	
NO.	DESCRIPTION

F.A.P. NO. AC-STBG-3241(5)E
 SHA NO. BC410024
 BALTIMORE CITY NO. TR17302



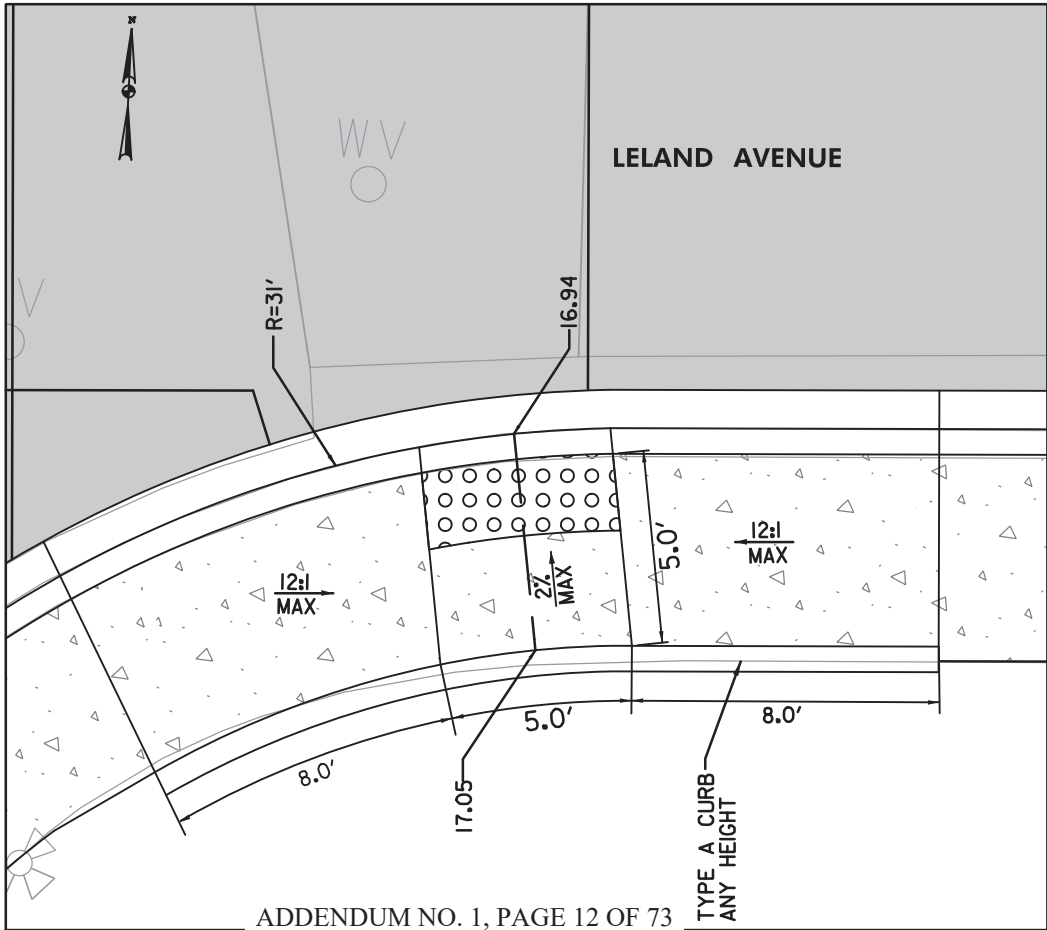
STA. 103+35, RT

DE-03

CITY OF BALTIMORE
 DEPARTMENT OF TRANSPORTATION
 CONTRACT NO. TR17302
 S. CLINTON STREET REHABILITATION
 FROM BOSTON STREET TO KEITH AVENUE
 DETAIL SHEET

SCALE: 1"=5'

DATE: JULY 2019
 SHEET OF

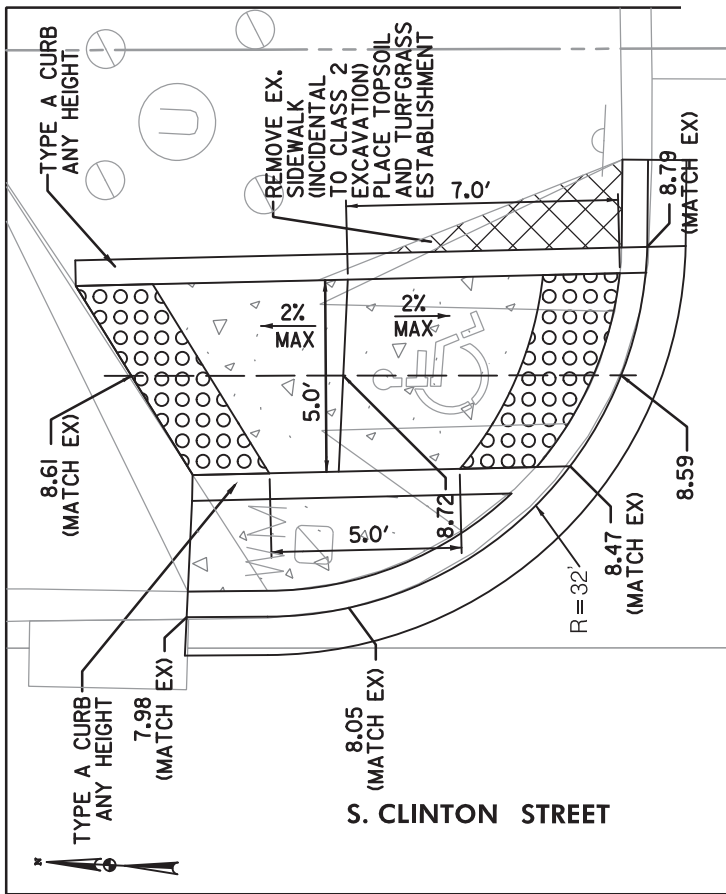


STA. 103+10, RT

ADDENDUM NO. 1, PAGE 12 OF 73
 291R

DRAWN BY _____
 CHECKED BY _____

NO.	DESCRIPTION	DATE	BY



F.A.P. NO. AC-STBG-3241(5)E
 SHA NO. BC410024
 BALTIMORE CITY NO. TR17302

DE-04

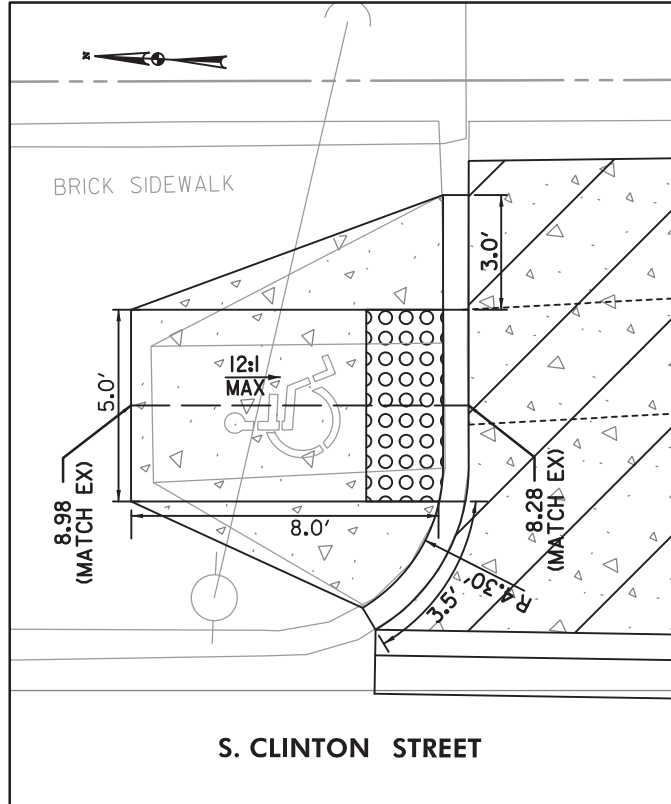
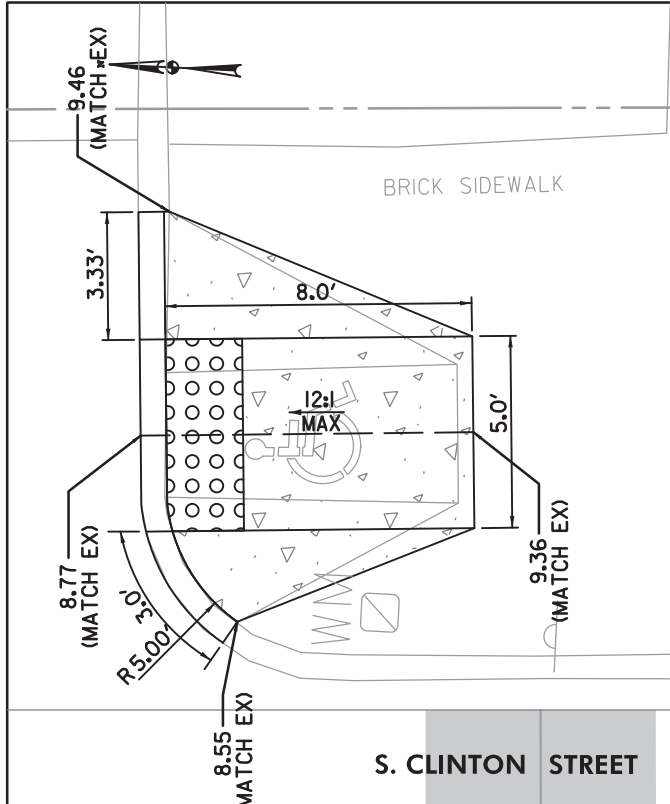
CITY OF BALTIMORE
 DEPARTMENT OF TRANSPORTATION
 CONTRACT NO. TR17302
 S. CLINTON STREET REHABILITATION
 FROM BOSTON STREET TO KEITH AVENUE
 DETAIL SHEET

SCALE: 1"=5'

DATE: JULY, 2019
 SHEET OF

* NOTES:

1. REMOVE AND SALVAGE EXISTING BRICKS.
2. FORM AND POUR PEDESTRIAN RAMP.
3. PLACE SALVAGED BRICKS AROUND THE PERIMETER OF THE CONCRETE PEDESTRIAN RAMP IN A SIMILAR PATTERN. SAWCUT SALVAGED BRICKS AS REQUIRED TO MATCH PATTERN.
4. REMOVING, SALVAGING, SAWCUTTING, AND PLACING BRICKS SHALL BE INCIDENTAL TO 5 INCH CONCRETE SIDEWALK.



DRAWN BY _____
 CHECKED BY _____

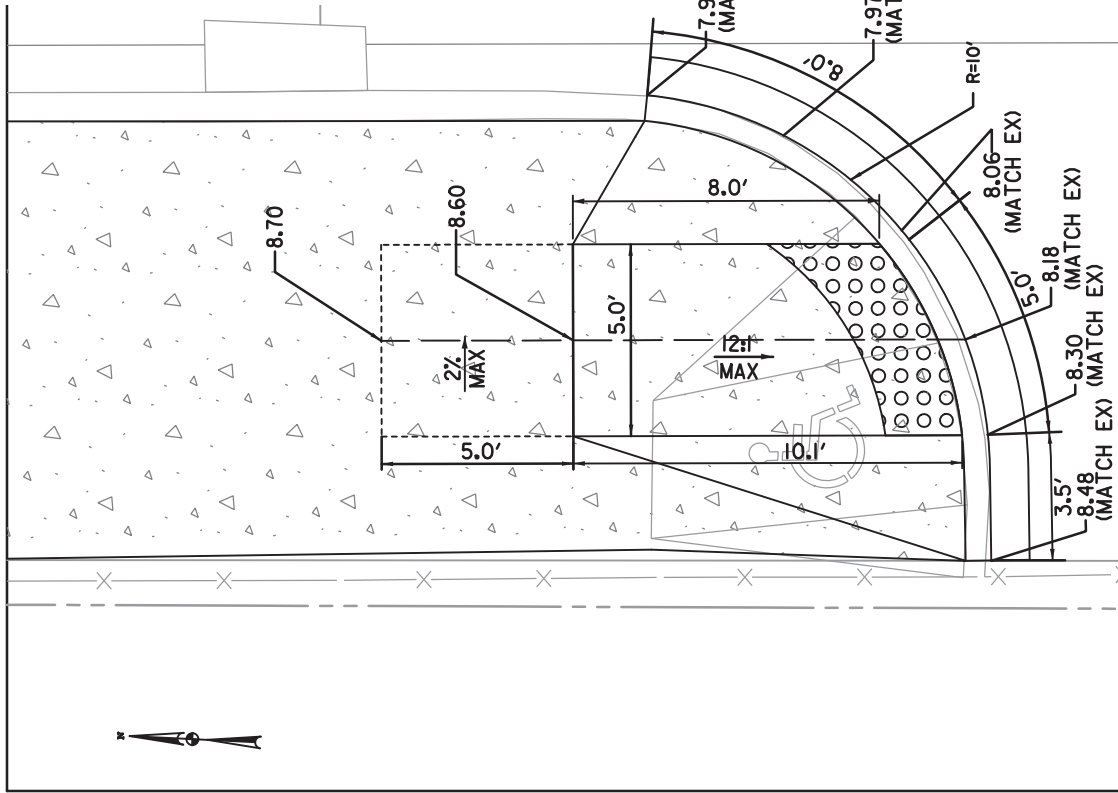


REVISIONS	
NO.	DESCRIPTION

DATE BY

F.A.P. NO. AC-STBG-3241(5)E
 SHA NO. BC410024
 BALTIMORE CITY NO. TR17302

CLINTON STREET



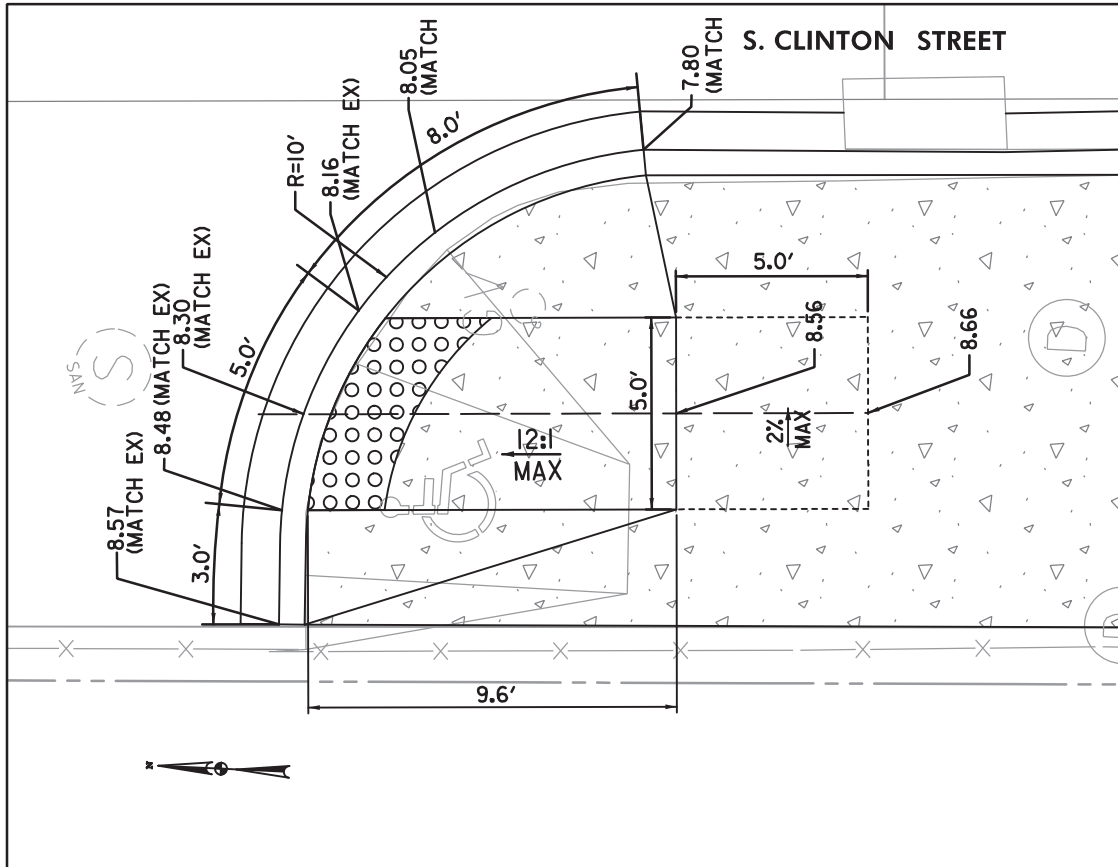
STA. 119+62, LT

DE-05

CITY OF BALTIMORE
 DEPARTMENT OF TRANSPORTATION
 CONTRACT No. TR17302
S. CLINTON STREET REHABILITATION
 FROM BOSTON STREET TO KEITH AVENUE
 DETAIL SHEET

SCALE: 1"=5'

DATE: JULY 2019
 SHEET OF

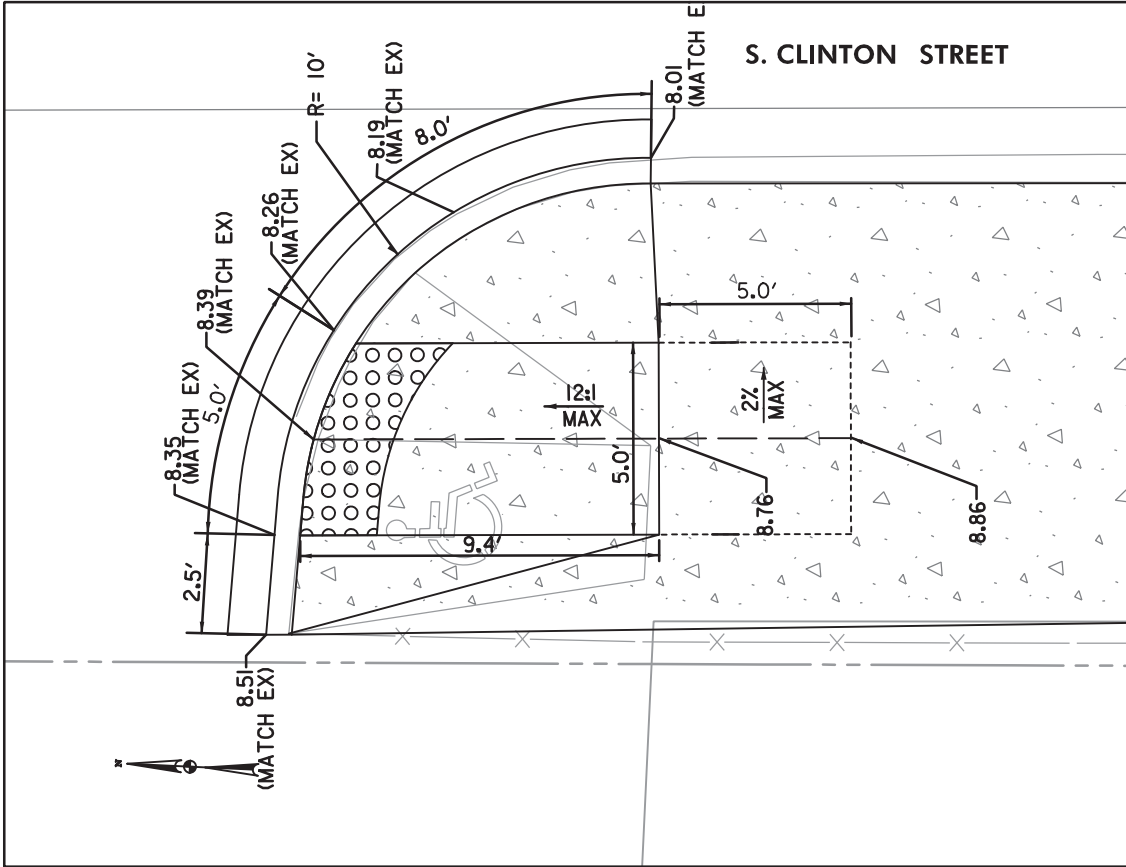
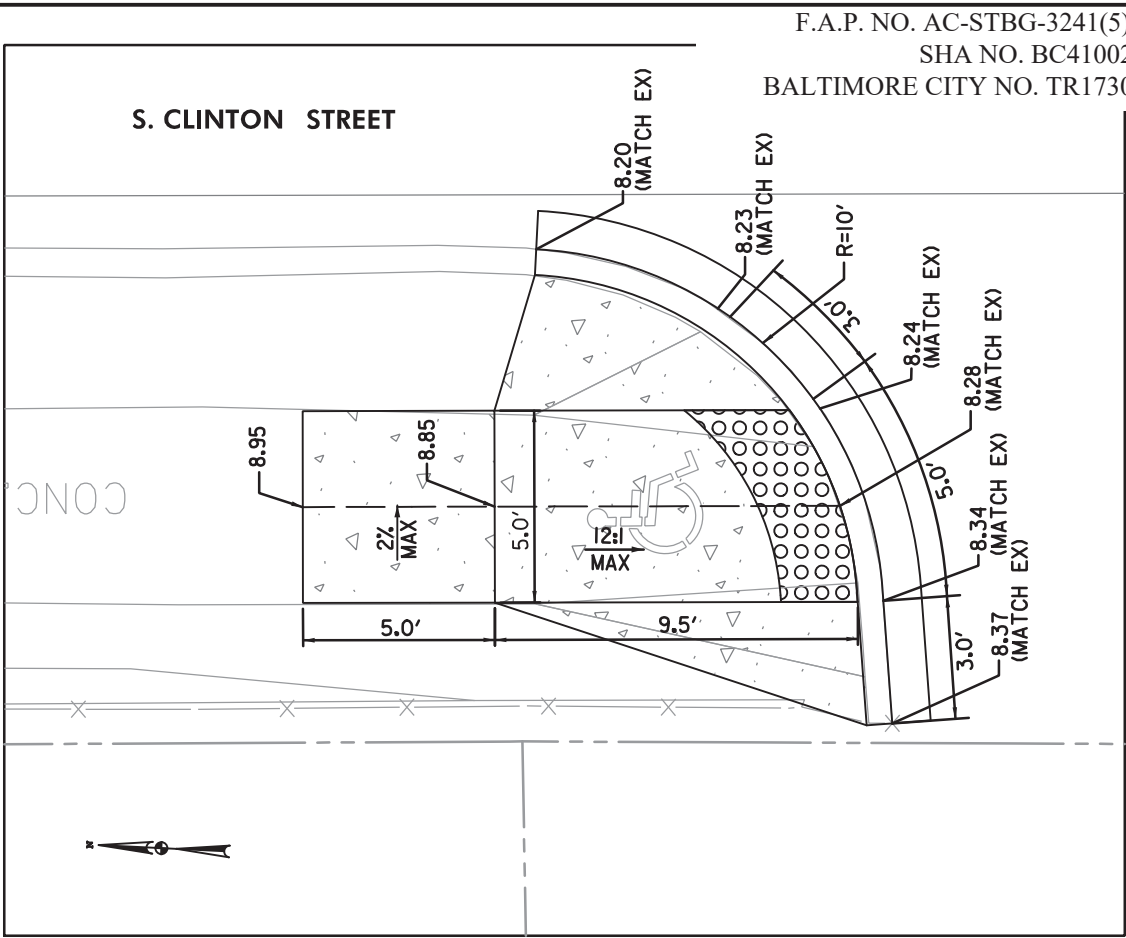


STA. 118+87, LT

DRAWN BY _____
 CHECKED BY _____

REVISIONS	
NO.	DESCRIPTION

NO.	DESCRIPTION	DATE	BY





\\\bdrv01\2014\2014\1511\Task 6 - Clinton Street\CADD\Plans\DE-F004_Clinton-St.dgn
 Wednesday, February 05, 2020 AT 04:08 PM

REVISIONS		DATE	BY
NO.	DESCRIPTION		

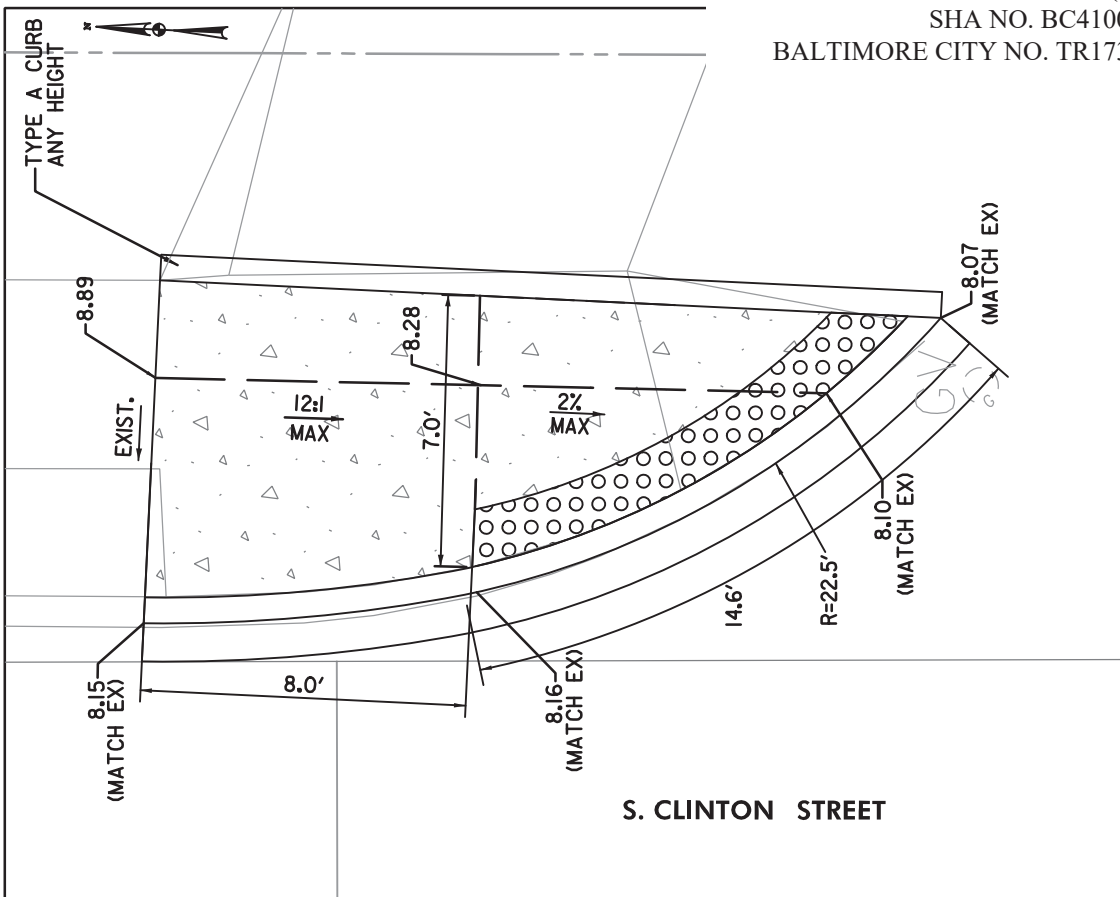
F.A.P. NO. AC-STBG-3241(5)E
 SHA NO. BC410024
 BALTIMORE CITY NO. TR17302

DE-07

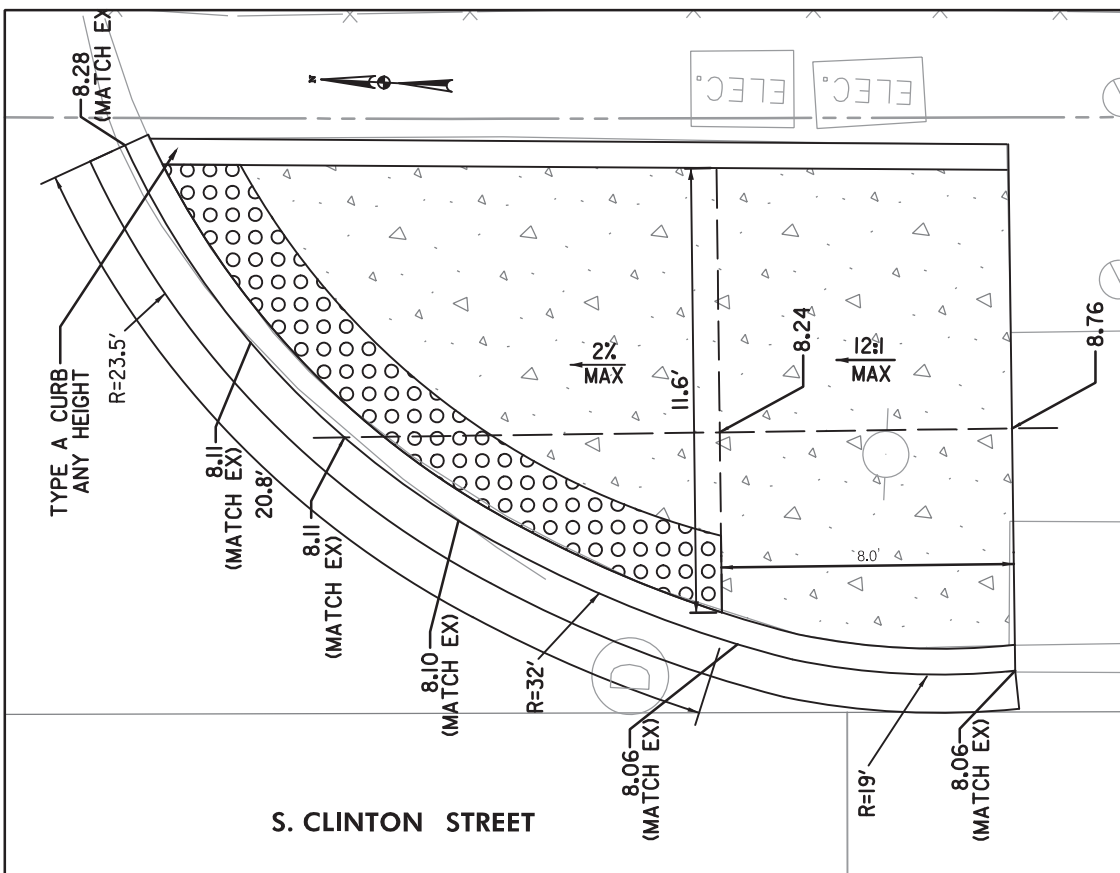
CITY OF BALTIMORE
 DEPARTMENT OF TRANSPORTATION
 CONTRACT No. TR17302
 S. CLINTON STREET REHABILITATION
 FROM BOSTON STREET TO KEITH AVENUE
 DETAIL SHEET

SCALE: 1"=5'

DATE: JULY 2019
 SHEET OF



STA. 120+49, RT



STA. 120+00, RT

ADDENDUM NO. 1, PAGE 16 OF 73
 295R

DRAWN BY _____
 CHECKED BY _____

NO.	REVISIONS	DESCRIPTION	DATE	BY

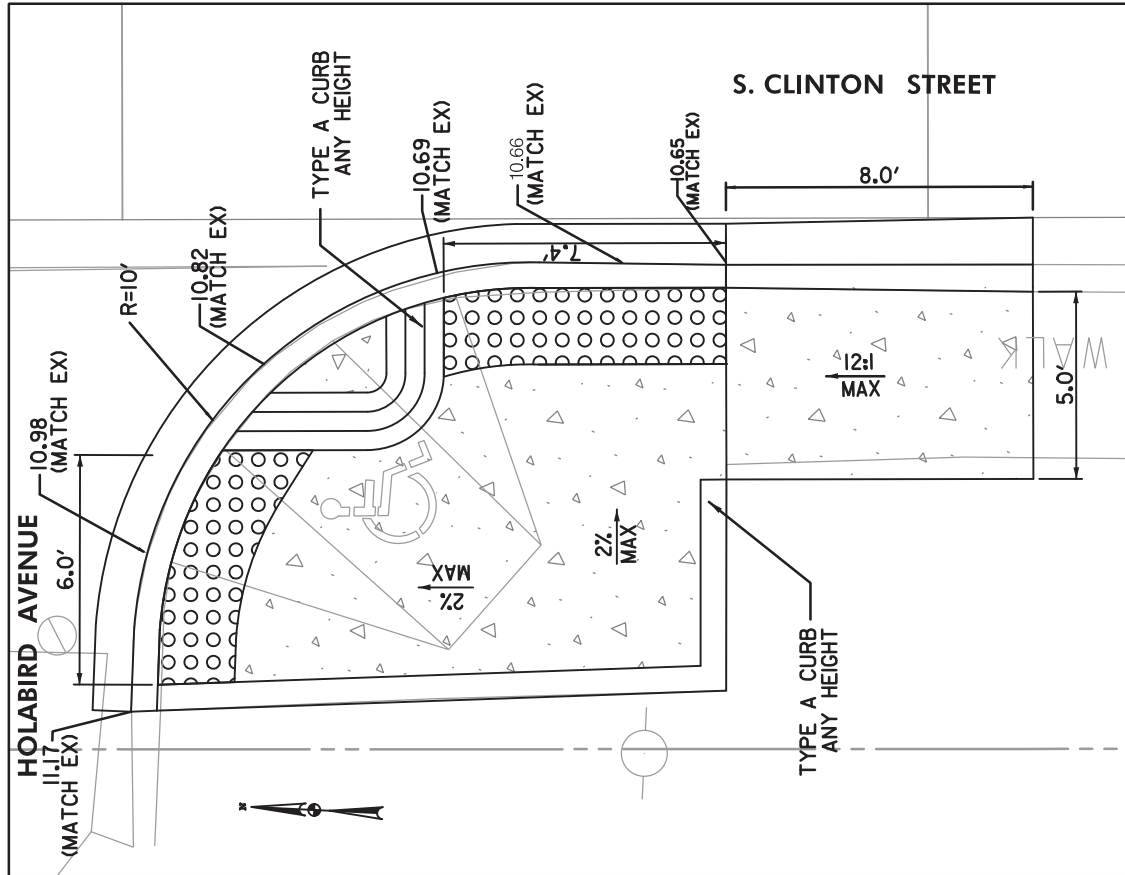
F.A.P. NO. AC-STBG-3241(5)E
 SHA NO. BC410024
 BALTIMORE CITY NO. TR17302

DE-08

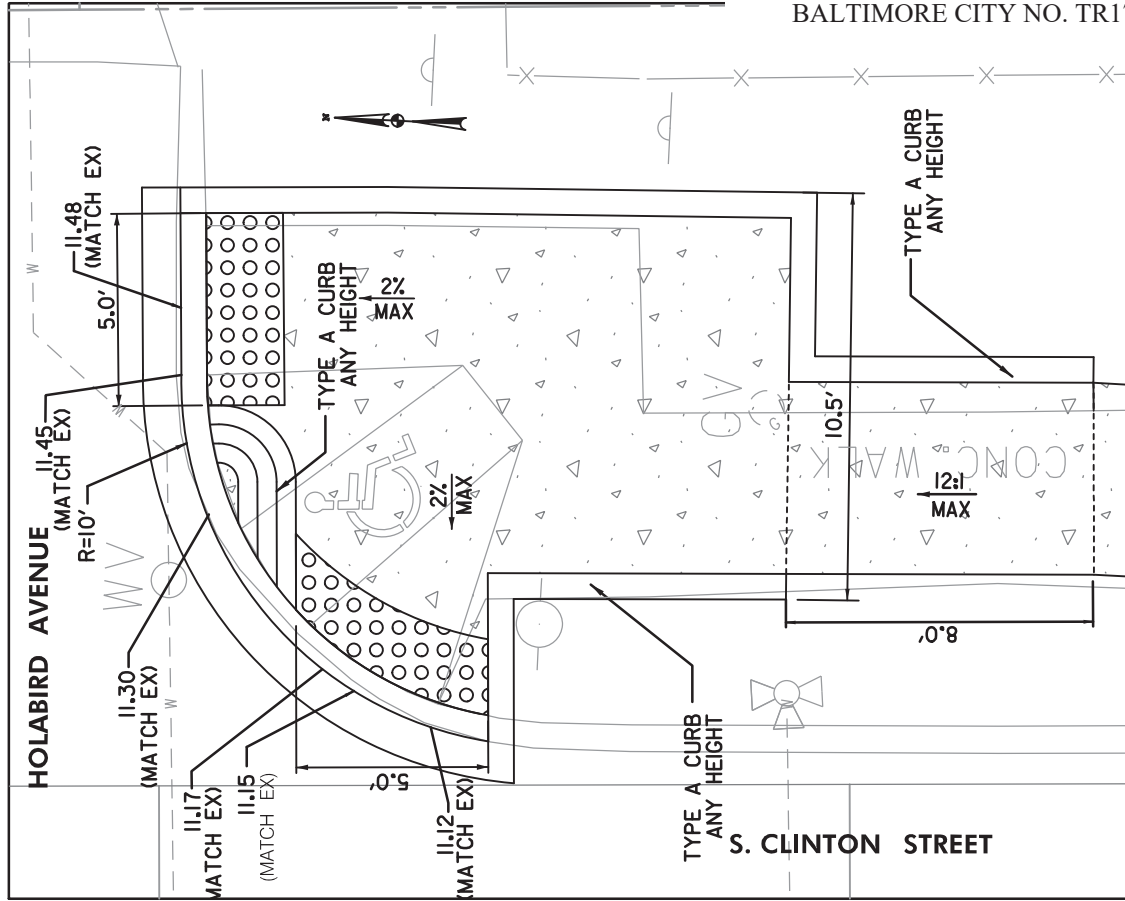
CITY OF BALTIMORE
 DEPARTMENT OF TRANSPORTATION
 CONTRACT NO. TR17302
 S. CLINTON STREET REHABILITATION
 FROM BOSTON STREET TO KEITH AVENUE
 DETAIL SHEET

SCALE: 1"=5'

DATE: JULY 2019
 SHEET OF



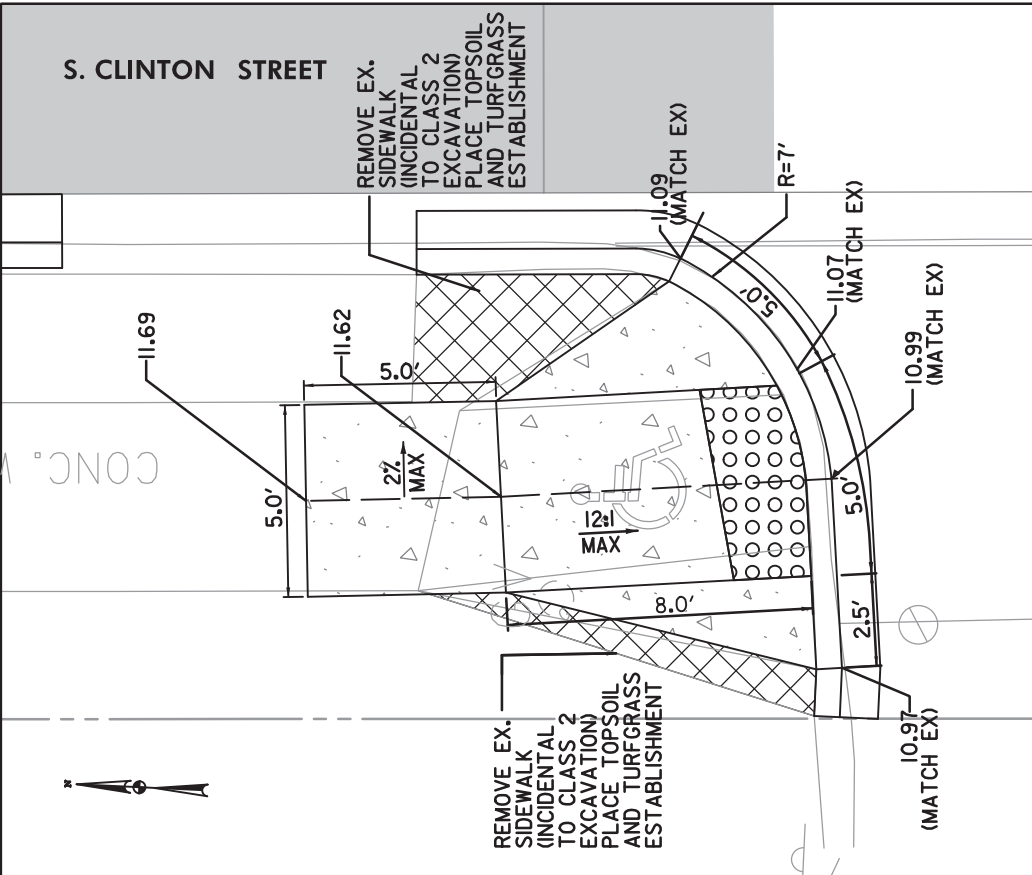
STA. 129+28, LT



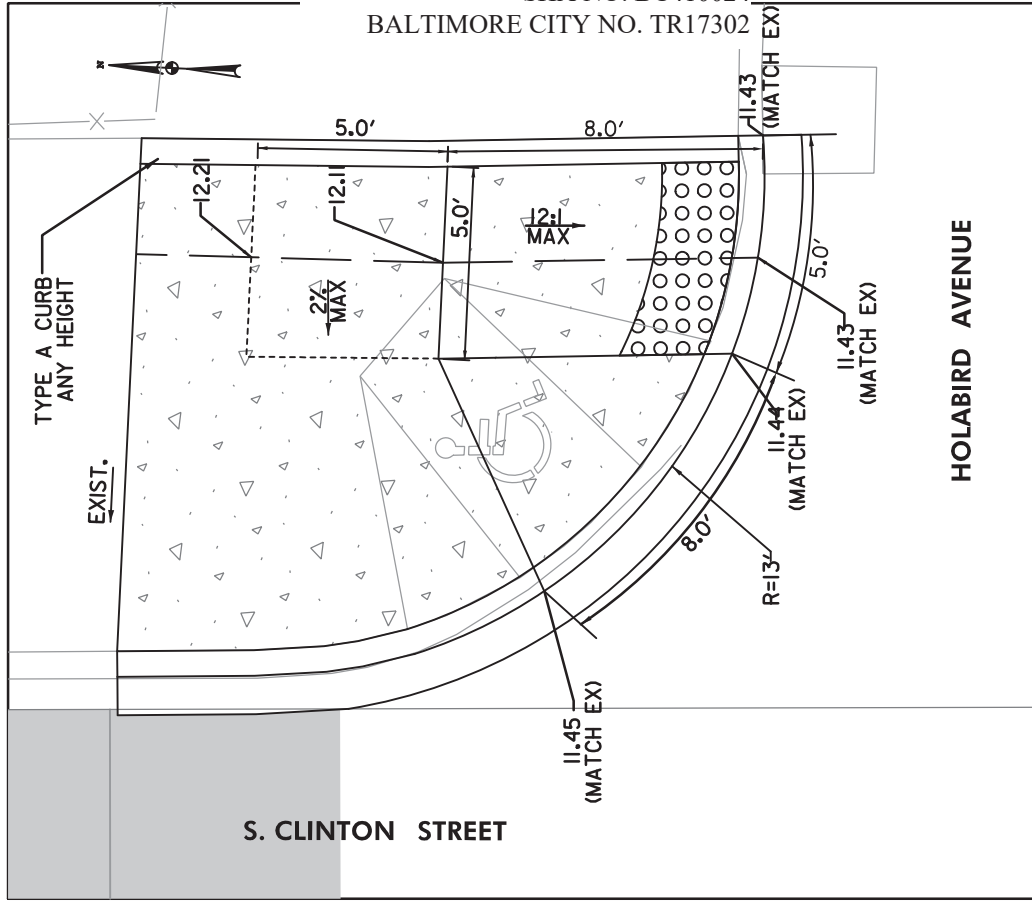
STA. 129+25, RT

DRAWN BY _____
 CHECKED BY _____

REVISIONS		DATE	BY
NO.	DESCRIPTION		



ADDENDUM NO. 1, PAGE 18 OF 73
297R



STA. 129+80, LT

STA. 129+73, RT

DE-09

CITY OF BALTIMORE
DEPARTMENT OF TRANSPORTATION
CONTRACT NO. TR17302
S. CLINTON STREET REHABILITATION
FROM BOSTON STREET TO KEITH AVENUE
DETAIL SHEET

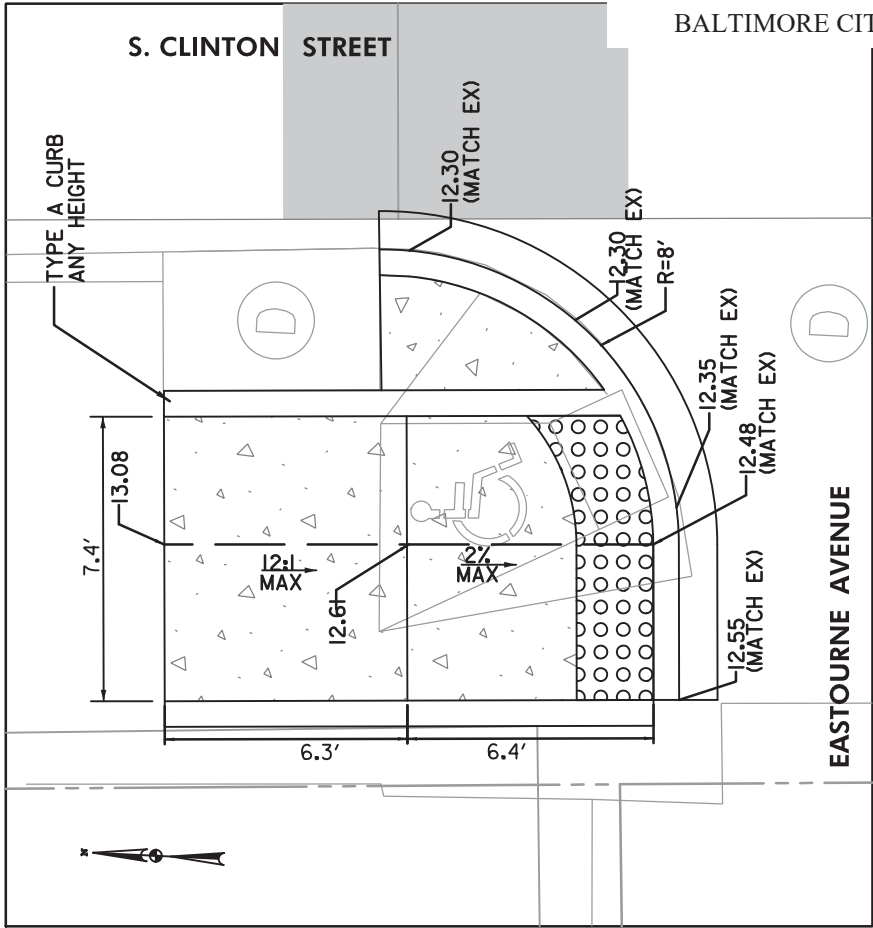
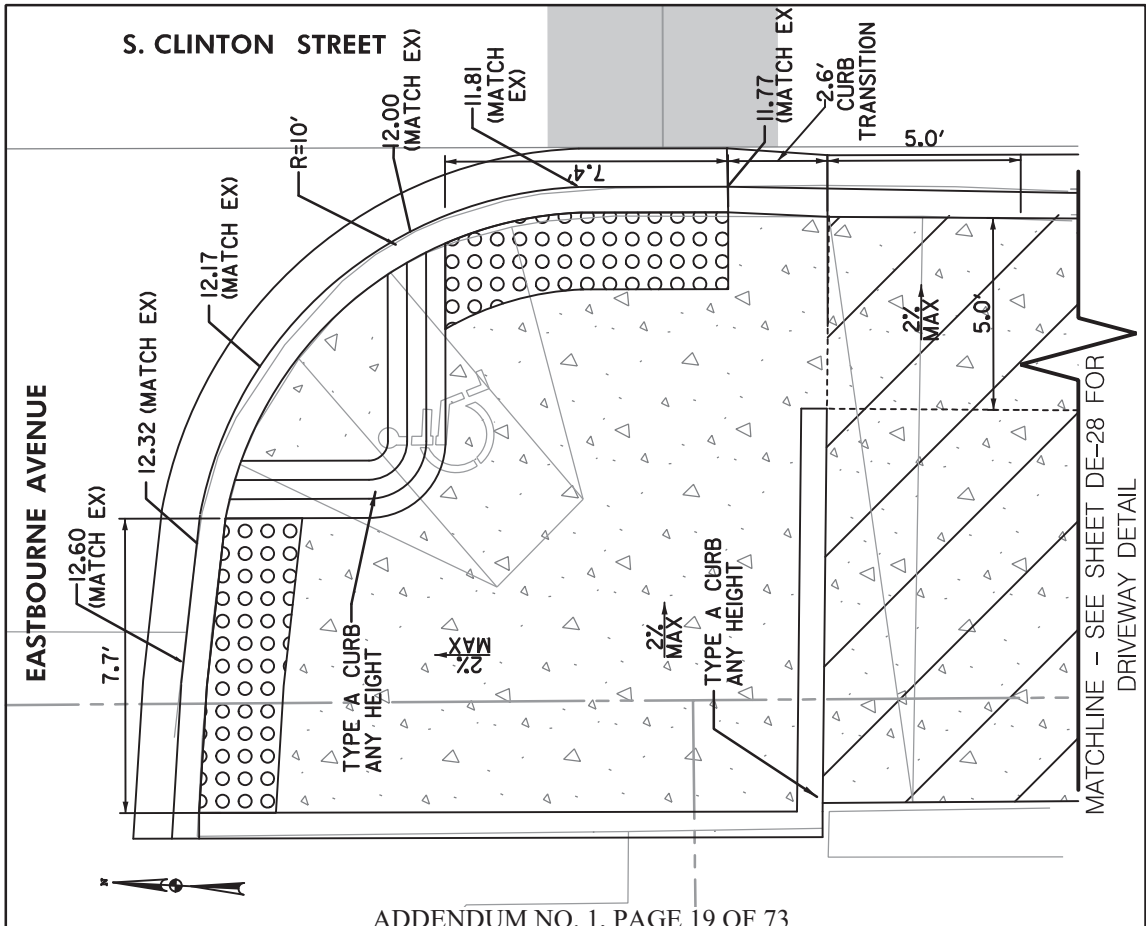
SCALE: 1"=5'

DATE: JULY 2019
SHEET OF



DRAWN BY _____
CHECKED BY _____

NO.	REVISIONS	DESCRIPTION	DATE	BY



F.A.P. NO. AC-STBG-3241(5)E
 SHA NO. BC410024
 BALTIMORE CITY NO. TR17302

DE-10

CITY OF BALTIMORE
 DEPARTMENT OF TRANSPORTATION
 CONTRACT No. TR17302
 S. CLINTON STREET REHABILITATION
 FROM BOSTON STREET TO KEITH AVENUE
 DETAIL SHEET

SCALE: 1"=5'

DATE: JULY 2019
 SHEET OF



ADDENDUM NO. 1, PAGE 19 OF 73
 298R

MATCHLINE - SEE SHEET DE-28 FOR
 DRIVEWAY DETAIL

STA. 134+54, LT

DRAWN BY _____
 CHECKED BY _____

NO.	DESCRIPTION	DATE	BY

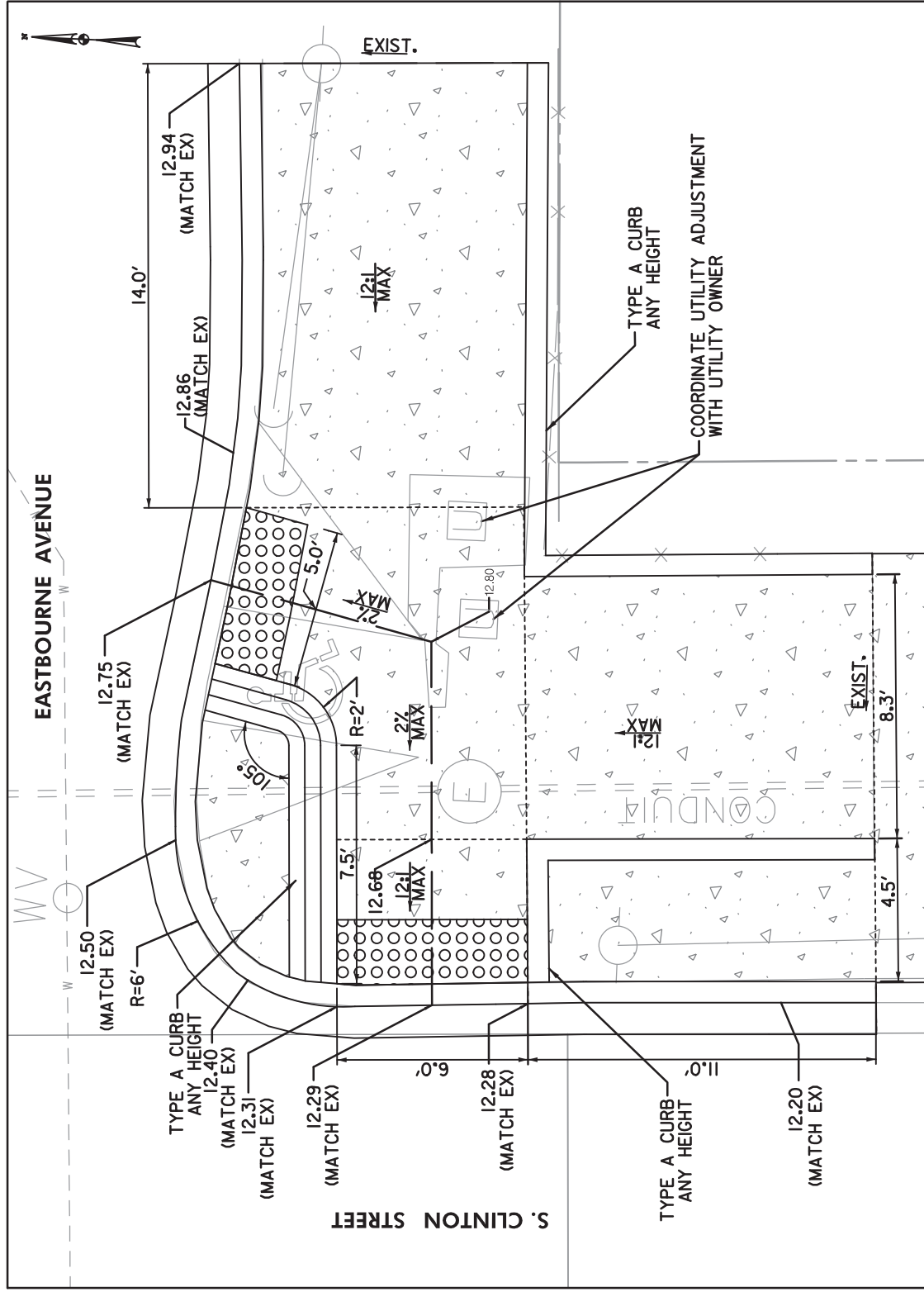
F.A.P. NO. AC-STBG-3241(5)E
 SHA NO. BC410024
 BALTIMORE CITY NO. TR17302

DE-II

CITY OF BALTIMORE
 DEPARTMENT OF TRANSPORTATION
 CONTRACT NO. TR17302
 S. CLINTON STREET REHABILITATION
 FROM BOSTON STREET TO KEITH AVENUE
 DETAIL SHEET

SCALE: 1"=5'

DATE: JULY, 2019
 SHEET OF



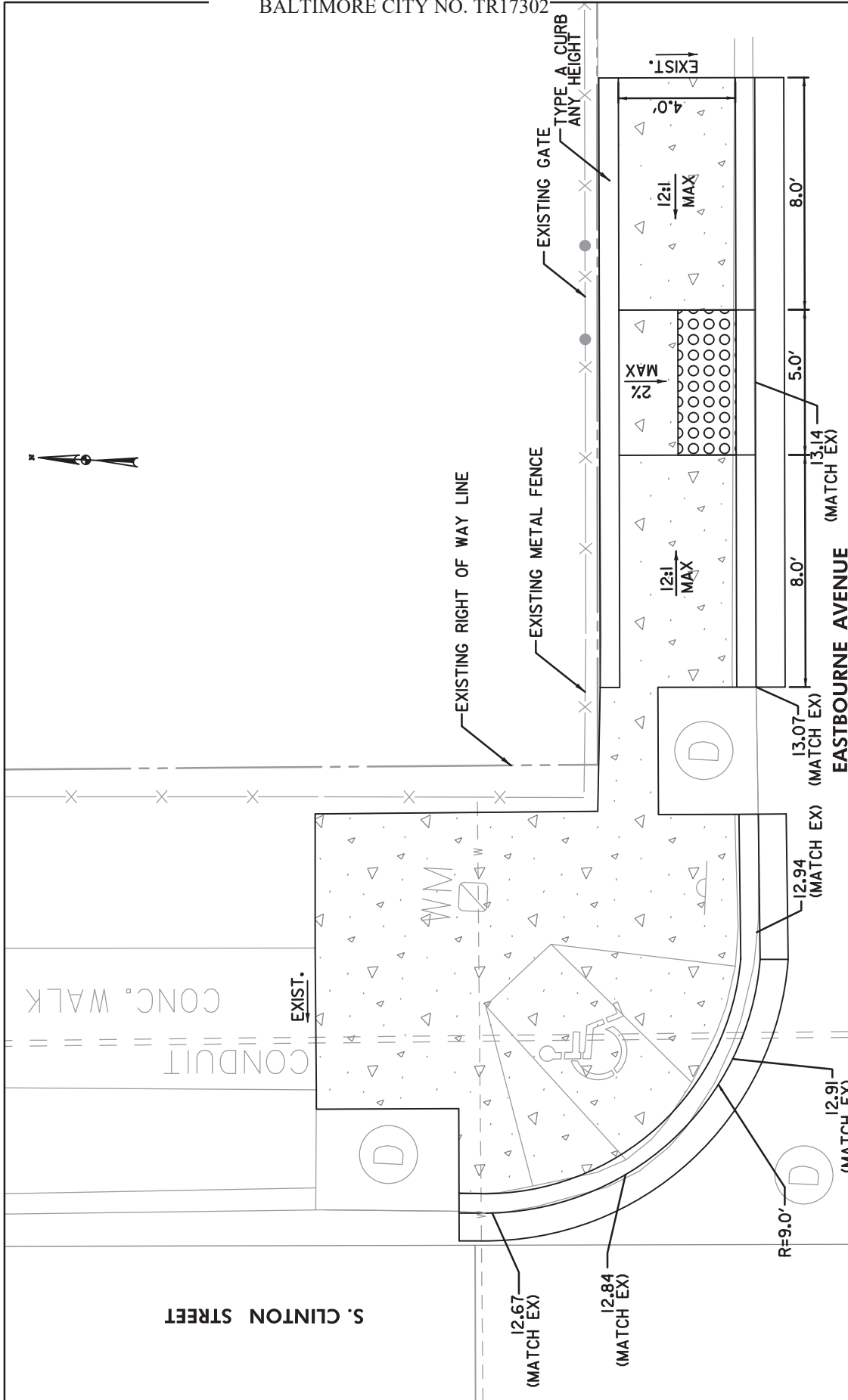
STA. 134+55, RT

DRAWN BY _____
 CHECKED BY _____

\\bdr\01\2014\2014\1451\BCL191\Task 6 - Clinton Street\CADD\Plans\PDF-P009\Clinton-St.dgn
 Wednesday, February 05, 2020 AT 04:08 PM

REVISIONS	
NO.	DESCRIPTION

F.A.P. NO. AC-STBG-3241(5)E
 SHA NO. BC410024
 BALTIMORE CITY NO. TR17302



DE-12

CITY OF BALTIMORE
 DEPARTMENT OF TRANSPORTATION
 CONTRACT No. TR17302
 S. CLINTON STREET REHABILITATION
 FROM BOSTON STREET TO KEITH AVENUE
 DETAIL SHEET



STA. 135+11, RT

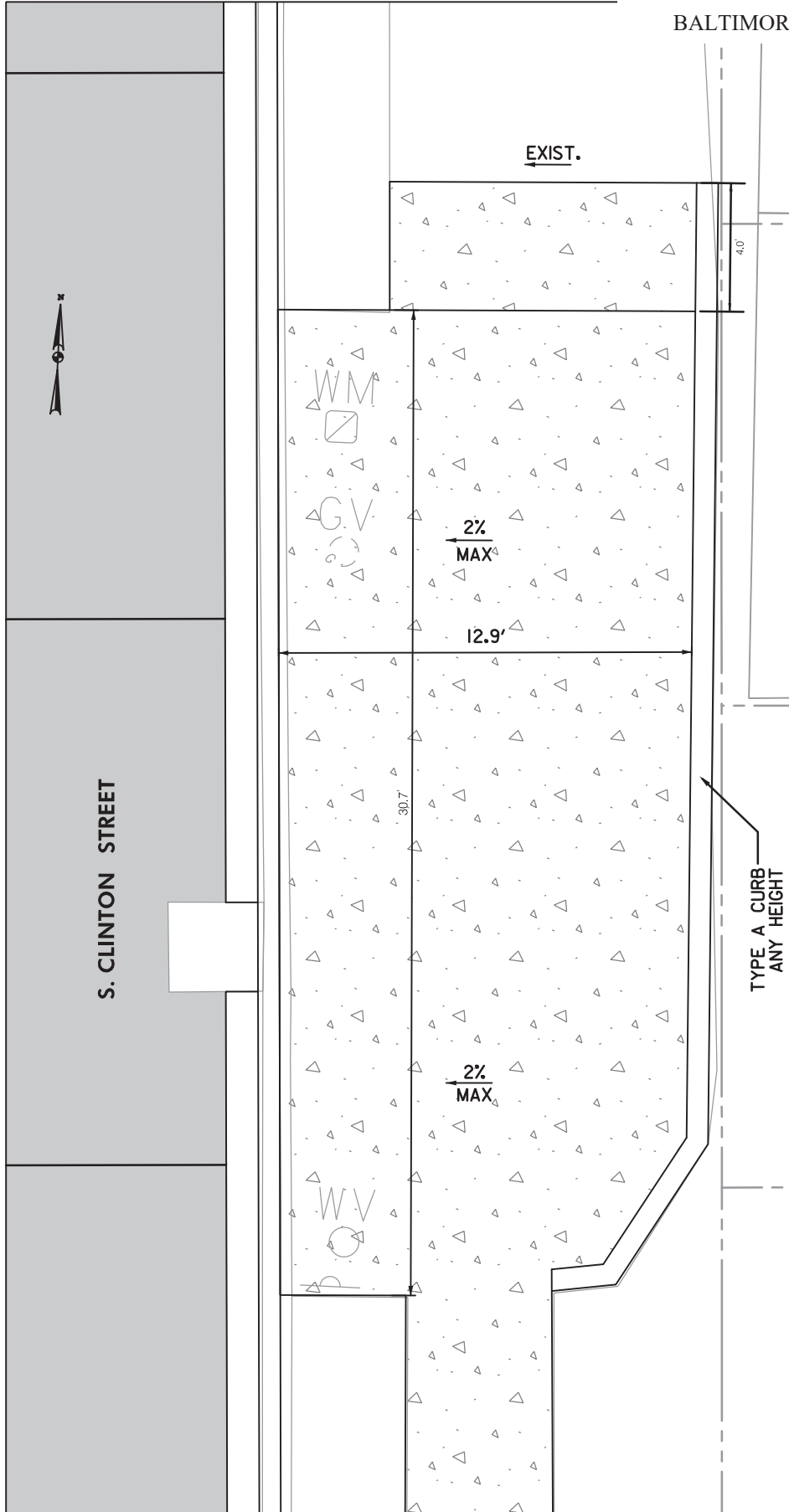
SCALE: 1"=5'

DATE: JULY 2019
 SHEET OF

ADDENDUM NO. 1, PAGE 21 OF 73
 300R

DRAWN BY _____
 CHECKED BY _____

REVISIONS		
NO.	DESCRIPTION	DATE BY



F.A.P. NO. AC-STBG-3241(5)E
 SHA NO. BC410024
 BALTIMORE CITY NO. TR17302

DE-13

CITY OF BALTIMORE
 DEPARTMENT OF TRANSPORTATION
 CONTRACT No. TR17302
 S. CLINTON STREET REHABILITATION
 FROM BOSTON STREET TO KEITH AVENUE
 DETAIL SHEET

SCALE: 1"=5'

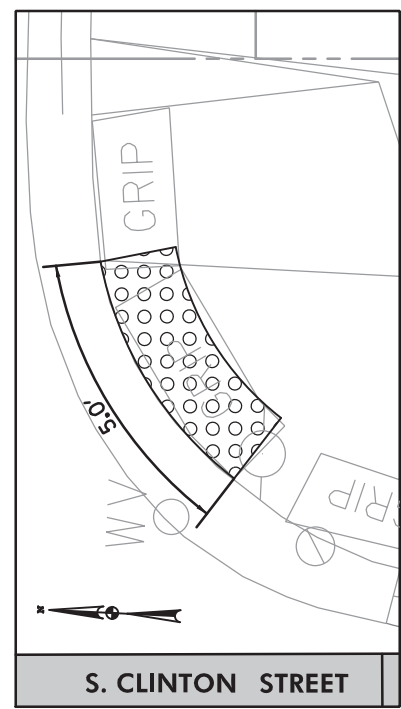
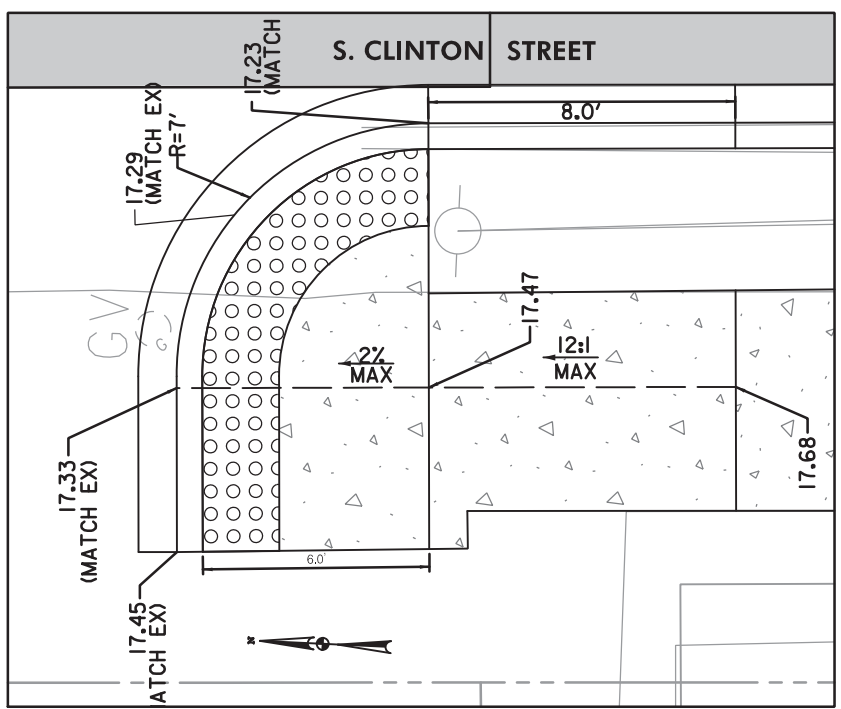
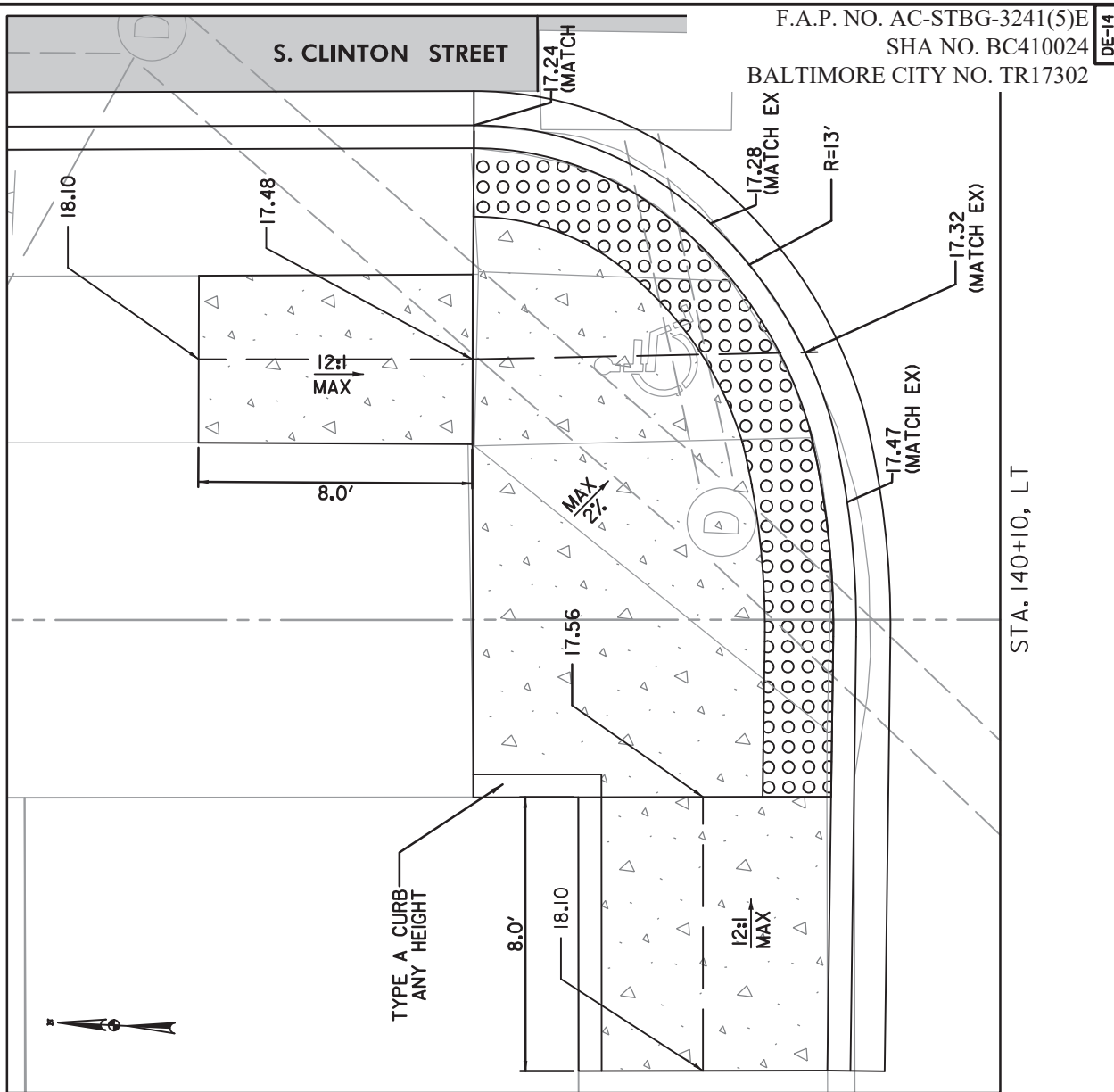
DATE: JULY 2019
 SHEET OF



BUS STOP DETAIL - STA. 139+37, RT

DRAWN BY _____
 CHECKED BY _____

REVISIONS		DATE	BY
NO.	DESCRIPTION		



DE-14

CITY OF BALTIMORE
DEPARTMENT OF TRANSPORTATION
CONTRACT NO. TR17302

S. CLINTON STREET REHABILITATION
FROM BOSTON STREET TO KEITH AVENUE

DETAIL SHEET

SCALE: 1"=5'

DATE: JULY 2019

SHEET OF



DRAWN BY _____

CHECKED BY _____

\\bdr\01\2014\2014\14151_BCI\191\Task 6 - Clinton Street\CADD\Plans\PDF-P012.Clinton-St.dgn
Wednesday, February 05, 2020 AT 04:08 PM

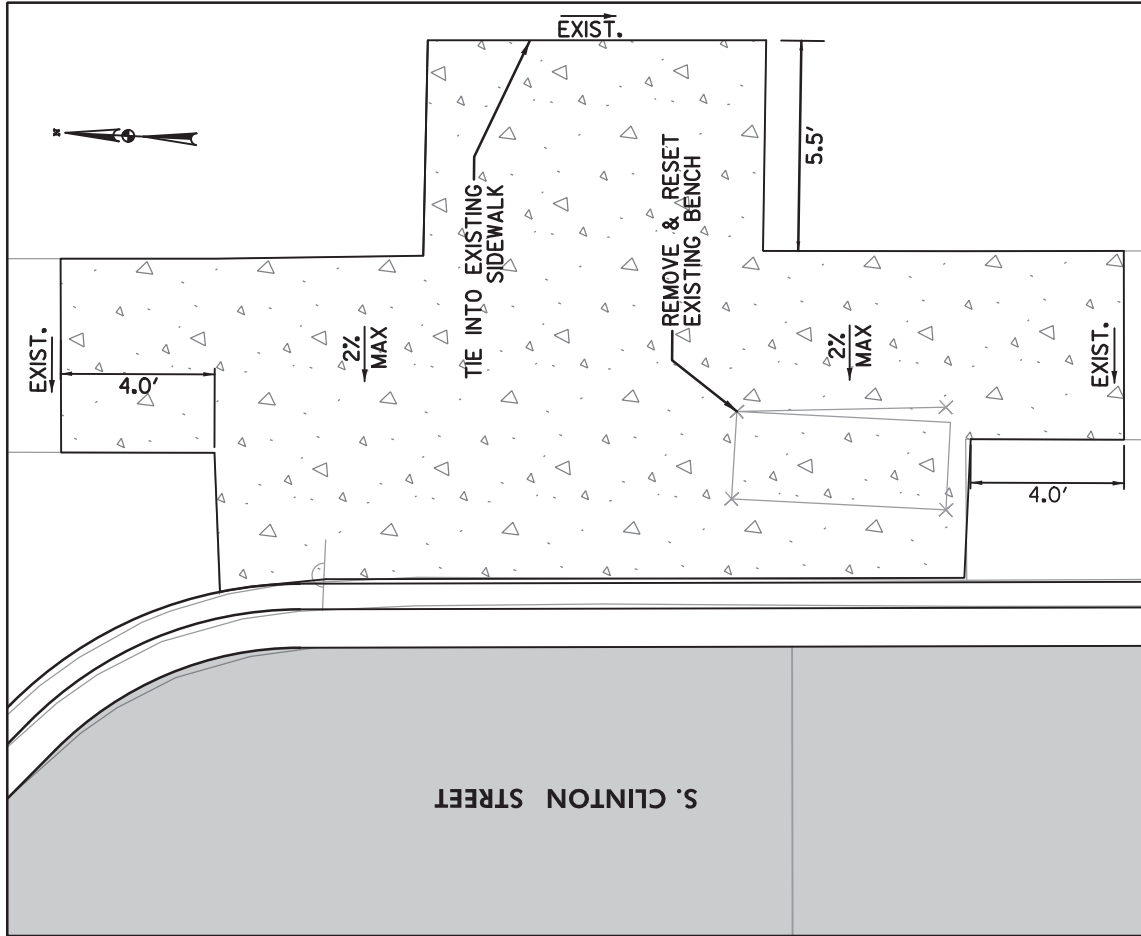
REVISIONS		
NO.	DESCRIPTION	DATE BY

F.A.P. NO. AC-STBG-3241(5)E
 SHA NO. BC410024
 BALTIMORE CITY NO. TR17302

DE-15

CITY OF BALTIMORE
 DEPARTMENT OF TRANSPORTATION
 CONTRACT No. TR17302
 S. CLINTON STREET REHABILITATION
 FROM BOSTON STREET TO KEITH AVENUE
 DETAIL SHEET

SCALE: 1"=5'
 DATE: JULY 2019
 SHEET OF



BUS STOP DETAIL - STA. 146+45, RT



DRAWN BY _____
 CHECKED BY _____

NO.	DESCRIPTION	DATE	BY

F.A.P. NO. AC-STBG-3241(5)E
 SHA NO. BC410024
 BALTIMORE CITY NO. TR17302

DE-16

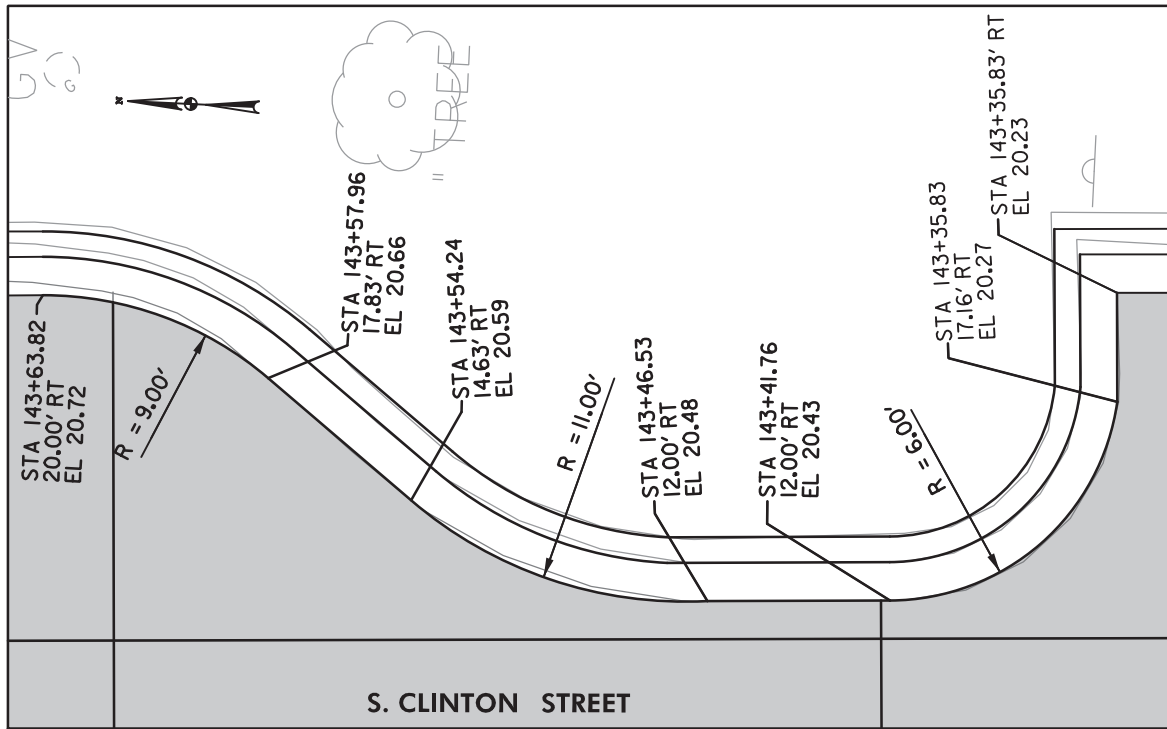
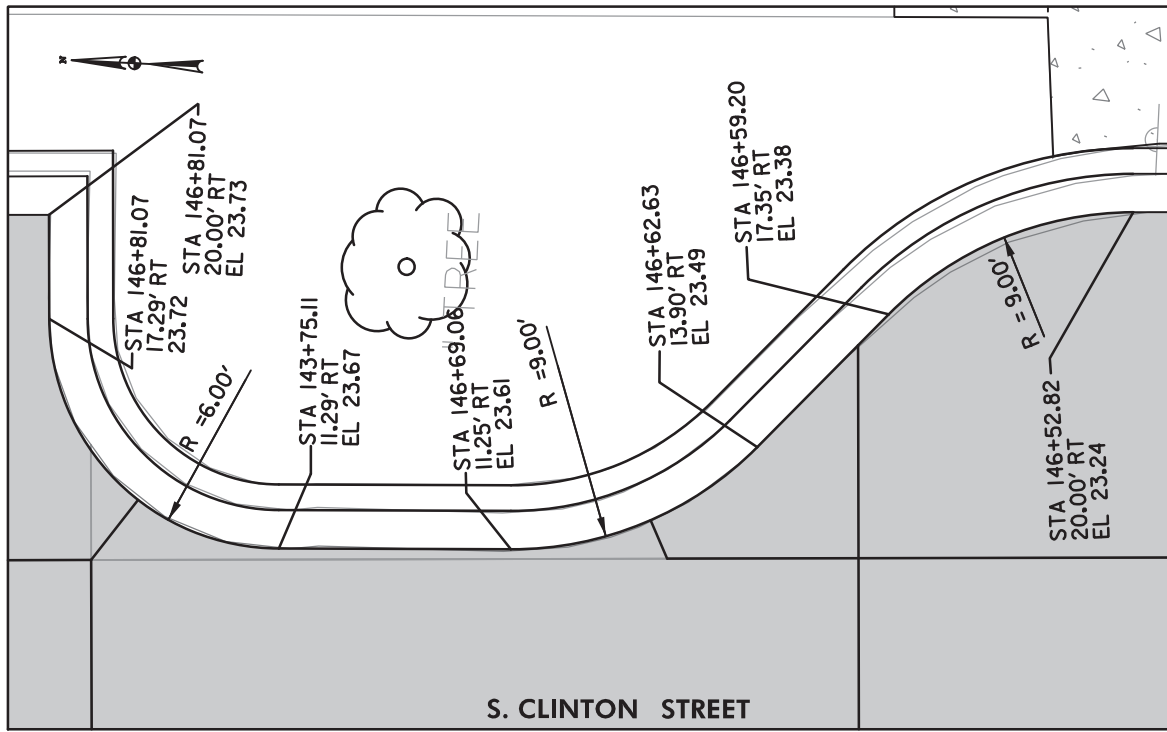
CITY OF BALTIMORE
 DEPARTMENT OF TRANSPORTATION
 CONTRACT No. TR17302
 S. CLINTON STREET REHABILITATION
 FROM BOSTON STREET TO KEITH AVENUE
 DETAIL SHEET

SCALE: 1"=5'

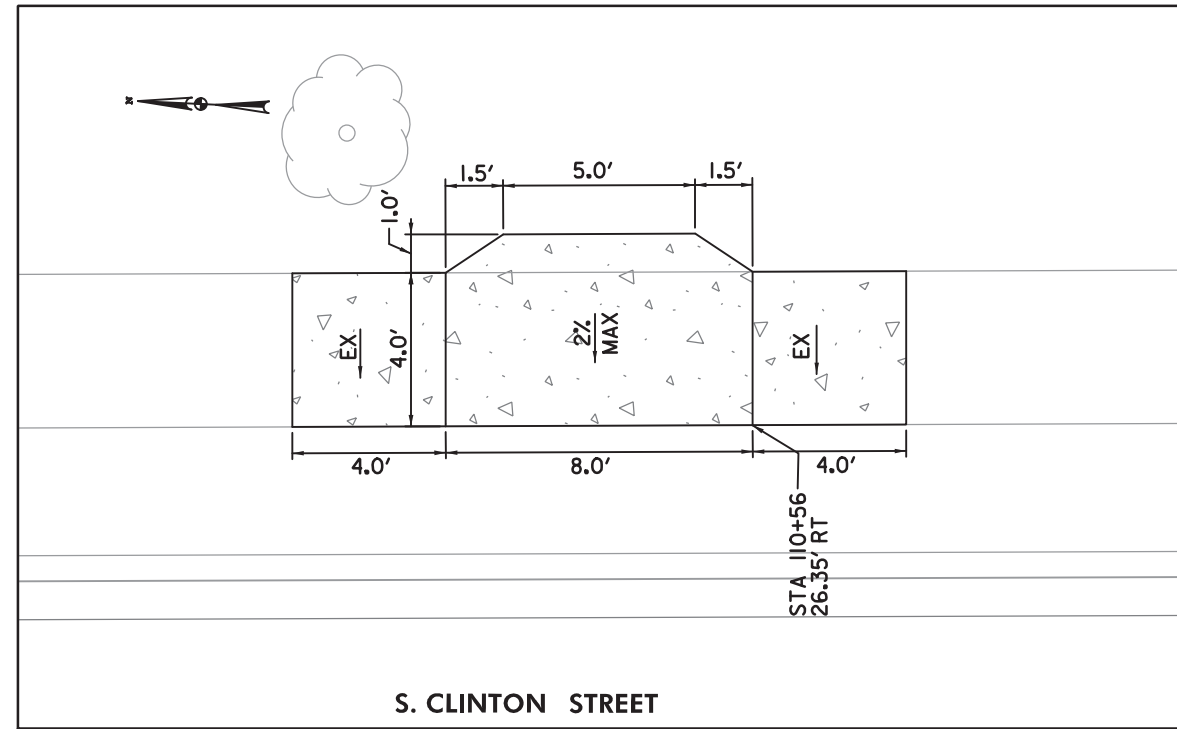
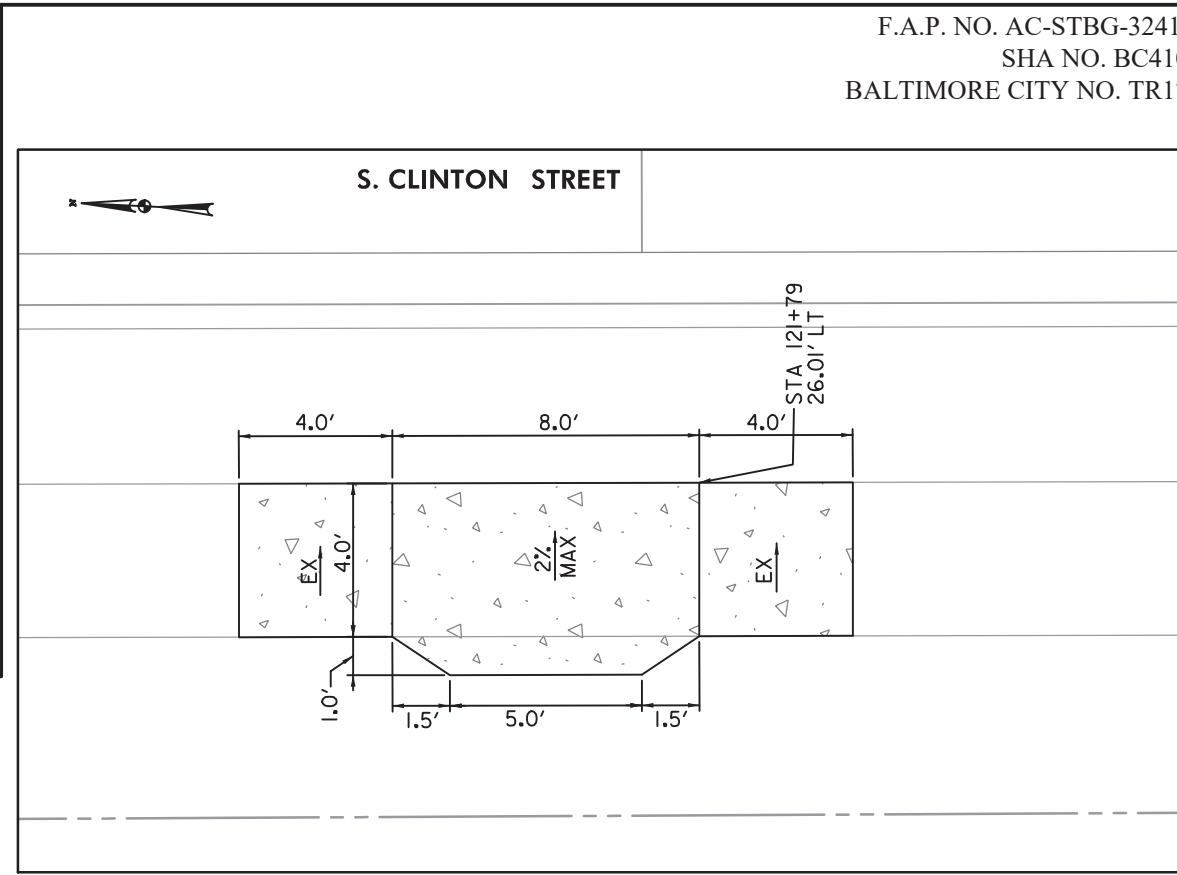
RK&K
 COMBINATION CURB AND GUTTER
 (NOT TO SCALE)
 12 INCH GUTTER PAN, 10 INCH DEPTH, 8 INCH HEIGHT

DRAWN BY _____
 CHECKED BY _____

DATE: JULY, 2019
 SHEET _____ OF _____



REVISIONS	
NO.	DESCRIPTION



F.A.P. NO. AC-STBG-3241(5)E
 SHA NO. BC410024
 BALTIMORE CITY NO. TR17302

DE-17

CITY OF BALTIMORE
 DEPARTMENT OF TRANSPORTATION
 CONTRACT No. TR17302
 S. CLINTON STREET REHABILITATION
 FROM BOSTON STREET TO KEITH AVENUE
 DETAIL SHEET

SCALE: 1"=5'

DATE: JULY 2019
 SHEET OF



STA. 121+80, LT

STA. 110+60, RT

STA 121+79
 26.01' LT

STA 110+56
 26.35' RT

DRAWN BY _____
 CHECKED BY _____

REVISIONS

NO.	DESCRIPTION	DATE	BY

F.A.P. NO. AC-STBG-3241(5)E
 SHA NO. BC410024
 BALTIMORE CITY NO. TR17302

DE-18

CITY OF BALTIMORE
 DEPARTMENT OF TRANSPORTATION
 CONTRACT No. TR17302
 S. CLINTON STREET REHABILITATION
 FROM BOSTON STREET TO KEITH AVENUE
 DETAIL SHEET

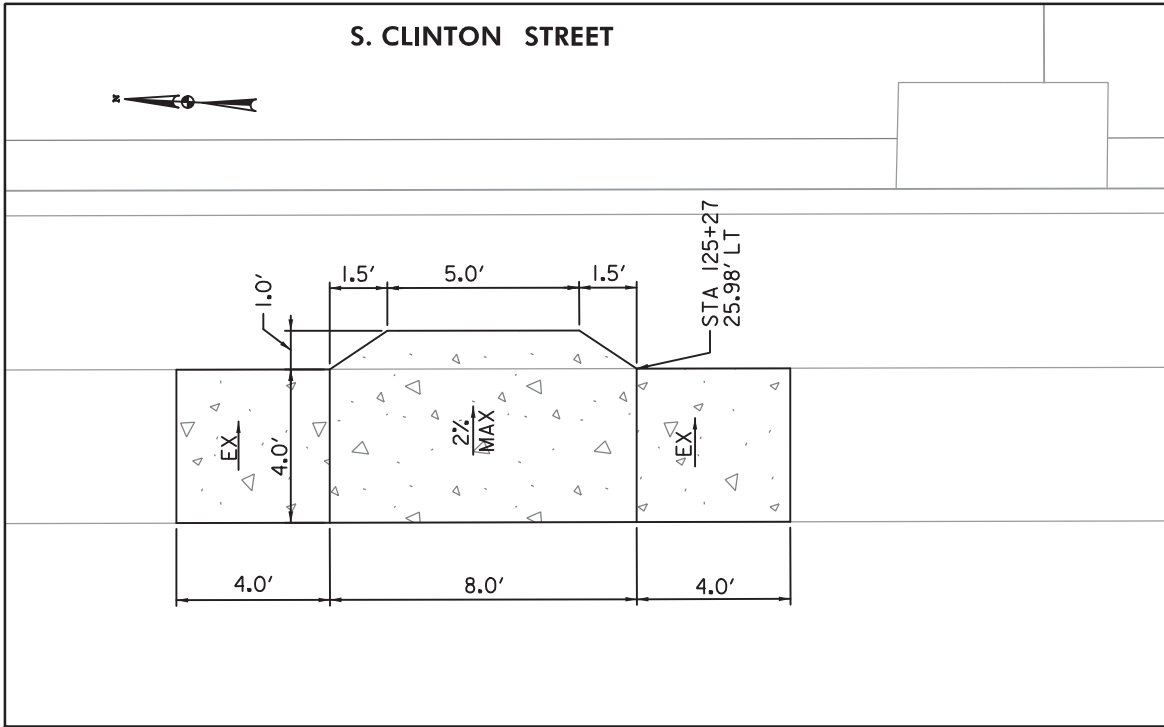
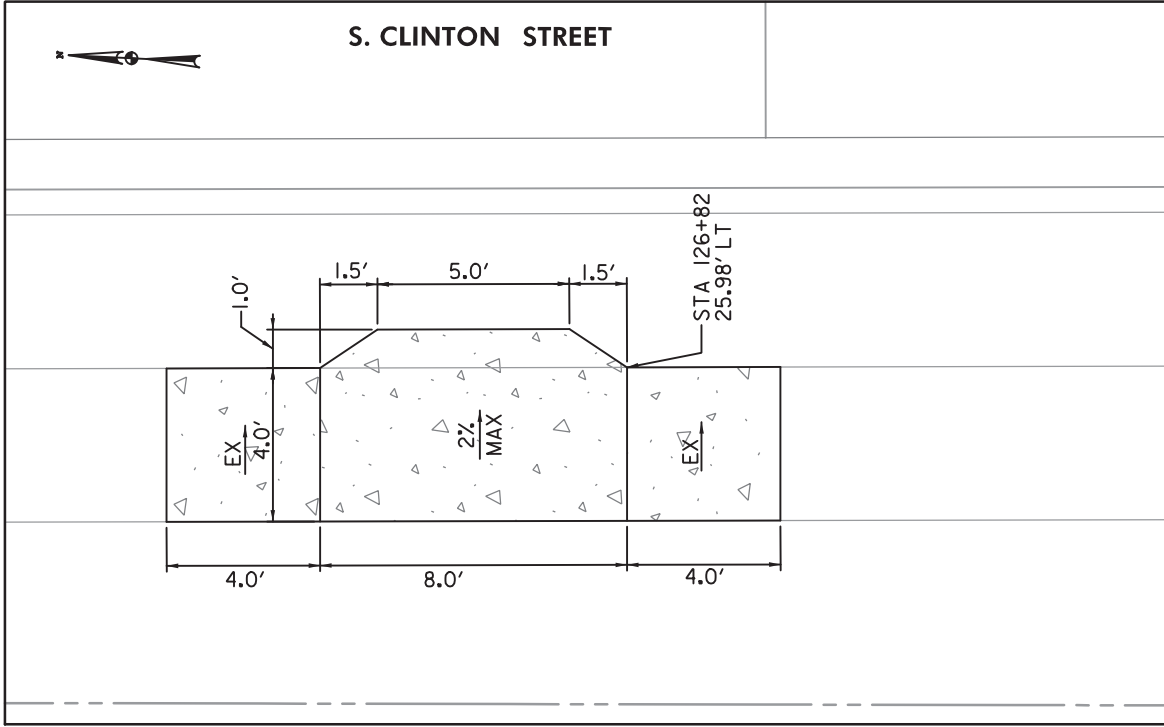
SCALE: 1"=5'

DATE: JULY 2019
 SHEET OF



STA. 126+85, LT

STA. 125+30, LT



DRAWN BY _____
 CHECKED BY _____

REVISIONS

NO.	DESCRIPTION	DATE	BY

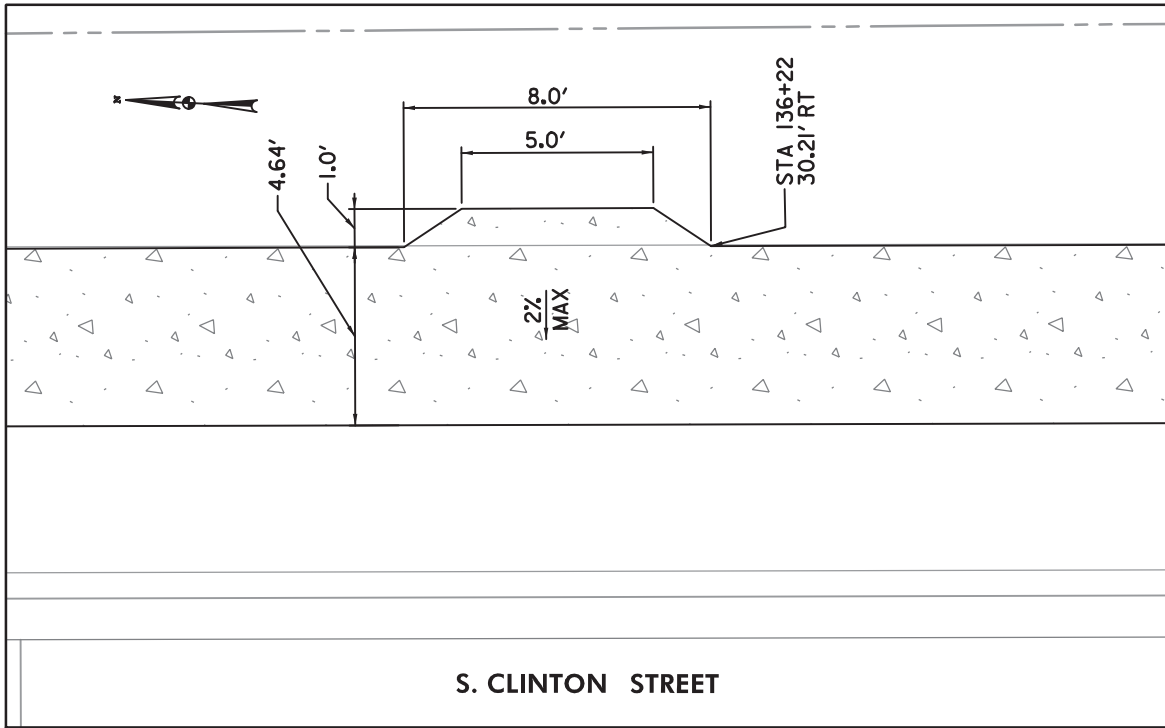
F.A.P. NO. AC-STBG-3241(5)E
 SHA NO. BC410024
 BALTIMORE CITY NO. TR17302

DE-19

CITY OF BALTIMORE
 DEPARTMENT OF TRANSPORTATION
 CONTRACT No. TR17302
 S. CLINTON STREET REHABILITATION
 FROM BOSTON STREET TO KEITH AVENUE
 DETAIL SHEET

SCALE: 1"=5'

DATE: JULY 2019
 SHEET OF

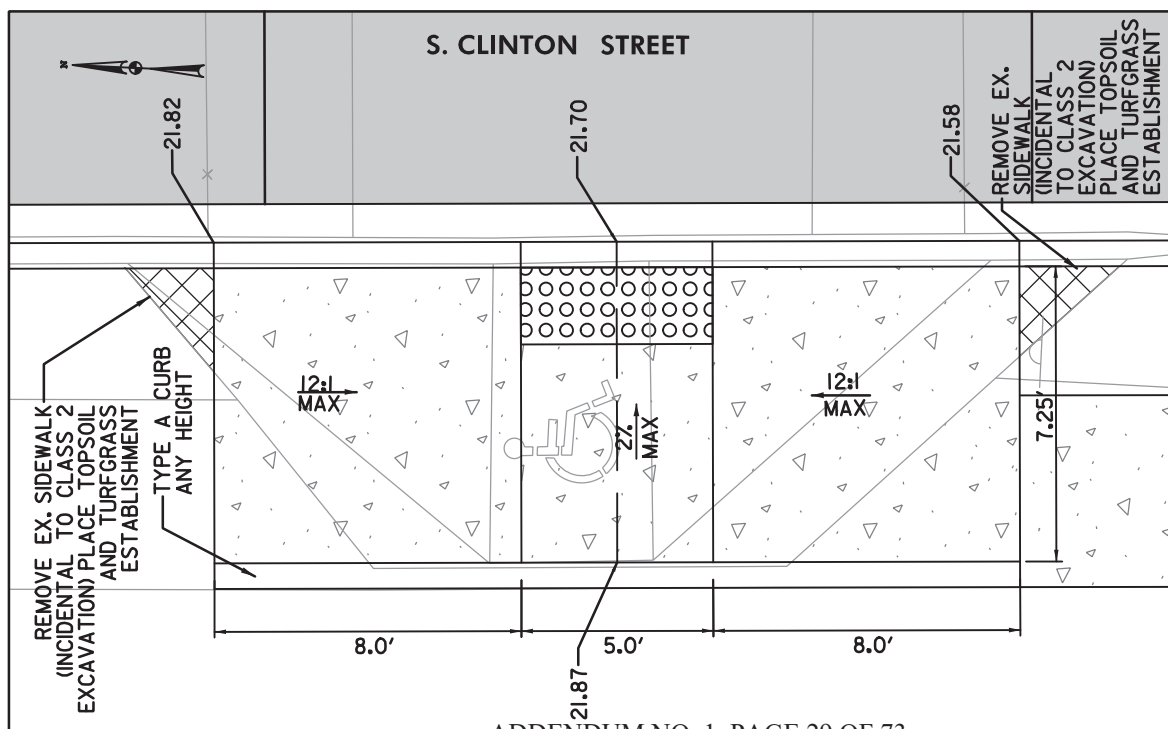
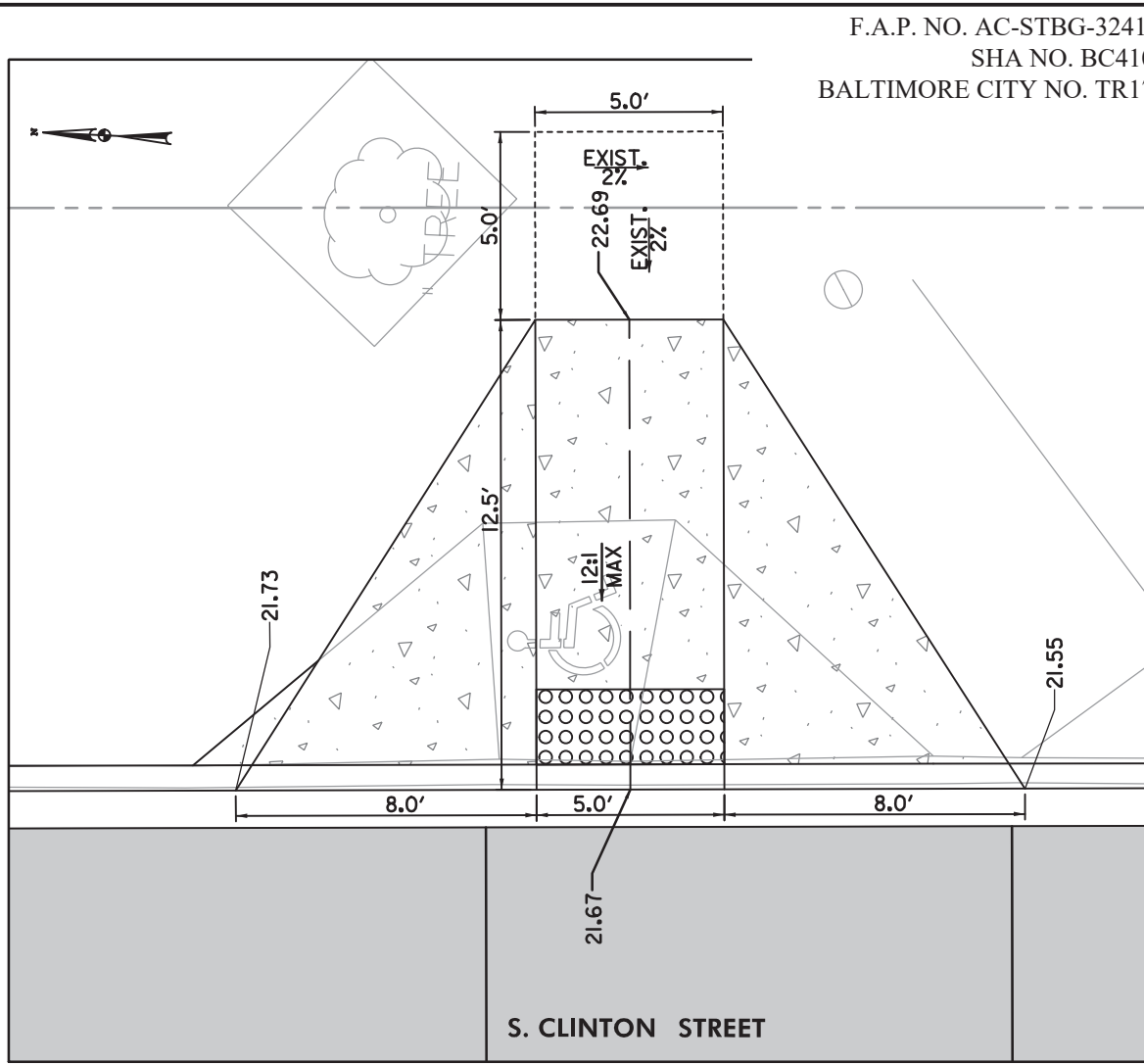


STA. 136+25, RT

S. CLINTON STREET

DRAWN BY _____
 CHECKED BY _____

NO.	REVISIONS	DATE	BY



F.A.P. NO. AC-STBG-3241(5)E
 SHA NO. BC410024
 BALTIMORE CITY NO. TR17302

DE-20

CITY OF BALTIMORE
 DEPARTMENT OF TRANSPORTATION
 CONTRACT NO. TR17302
 S. CLINTON STREET REHABILITATION
 FROM BOSTON STREET TO KEITH AVENUE
 DETAIL SHEET

SCALE: 1"=5'



DATE: JULY 2019
 SHEET OF

STA. 144+50, RT

STA. 145+60, LT

ADDENDUM NO. 1, PAGE 29 OF 73

308R

DRAWN BY _____
 CHECKED BY _____

REVISIONS	
NO.	DESCRIPTION

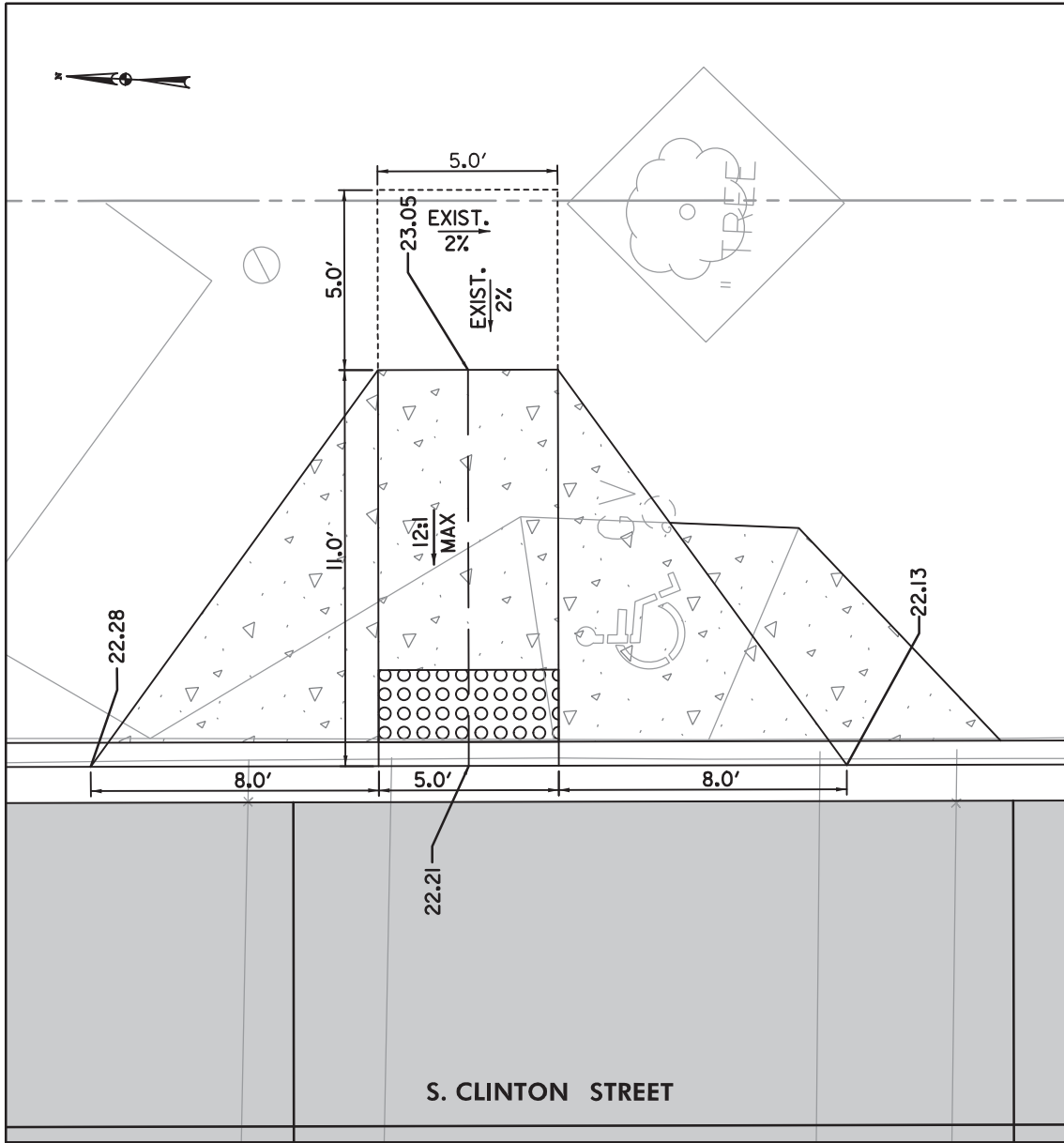
F.A.P. NO. AC-STBG-3241(5)E
 SHA NO. BC410024
 BALTIMORE CITY NO. TR17302

DE-21

CITY OF BALTIMORE
 DEPARTMENT OF TRANSPORTATION
 CONTRACT No. TR17302
 S. CLINTON STREET REHABILITATION
 FROM BOSTON STREET TO KEITH AVENUE
 DETAIL SHEET

SCALE: 1"=5'

DATE: JULY 2019
 SHEET OF



STA. 145+60, RT

DRAWN BY _____
 CHECKED BY _____

NO.	DESCRIPTION	DATE	BY

F.A.P. NO. AC-STBG-3241(5)E
 SHA NO. BC410024
 BALTIMORE CITY NO. TR17302

DE-22

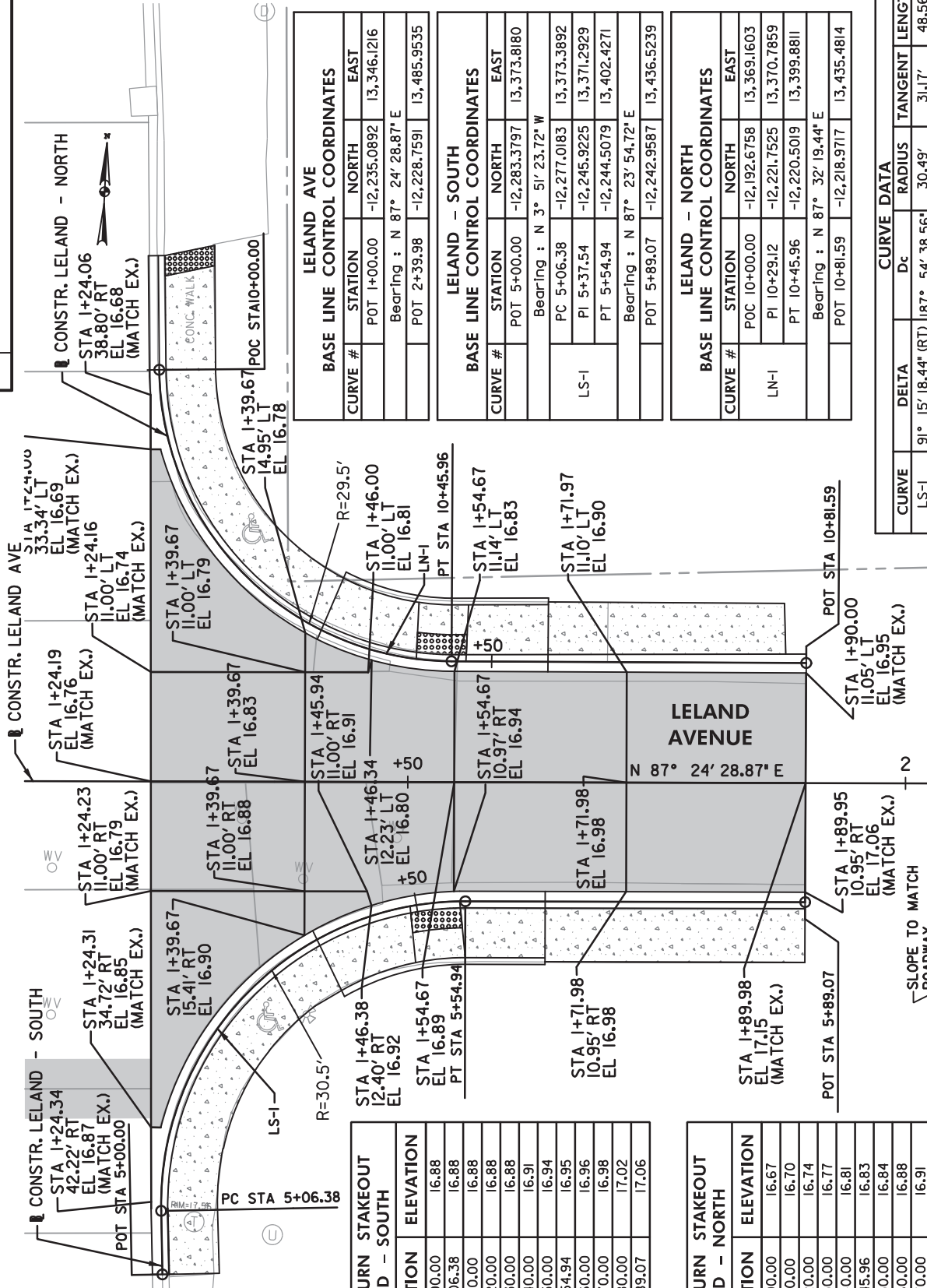
CITY OF BALTIMORE
 DEPARTMENT OF TRANSPORTATION
 CONTRACT No. TR17302
 S. CLINTON STREET REHABILITATION
 FROM BOSTON STREET TO KEITH AVENUE
 INTERSECTION DETAIL

SCALE: 1"=15'

DATE: JULY 2019
 SHEET OF



S. CLINTON STREET



BASE LINE CONTROL COORDINATES
LELAND AVE

CURVE #	STATION	NORTH	EAST
POT	1+00.00	-12,235.0892	13,346.1216
Bearing : N 87° 24' 28.87" E			
POT	2+39.98	-12,228.7591	13,485.9535

BASE LINE CONTROL COORDINATES
LELAND - SOUTH

CURVE #	STATION	NORTH	EAST
POT	5+00.00	-12,283.3797	13,373.8180
Bearing : N 3° 51' 23.72" W			
PC	5+06.38	-12,277.0183	13,373.3892
PI	5+37.54	-12,245.9225	13,371.2929
PT	5+54.94	-12,244.5079	13,402.4271
Bearing : N 87° 23' 54.72" E			
POT	5+89.07	-12,242.9587	13,436.5239

BASE LINE CONTROL COORDINATES
LELAND - NORTH

CURVE #	STATION	NORTH	EAST
POC	10+00.00	-12,192.6758	13,369.1603
PI	10+29.12	-12,221.7525	13,370.7859
PT	10+45.96	-12,220.5019	13,399.8811
Bearing : N 87° 32' 19.44" E			
POT	10+81.59	-12,218.9717	13,435.4814

CURVE DATA

CURVE	DELTA	RADIUS	TANGENT	LENGTH	EXTERNAL
LS-1	91° 15' 18.44" (RT)	187° 54' 38.56"	30.49'	31.17'	48.56'
LN-1	89° 15' 40.83" (LT)	194° 13' 22.73"	29.50'	29.12'	45.96'

CURB RETURN STAKEOUT
LELAND - SOUTH

POINT	STATION	ELEVATION
POT	5+00.00	16.88
PC	5+06.38	16.88
	5+10.00	16.88
	5+20.00	16.88
	5+30.00	16.88
	5+40.00	16.91
	5+50.00	16.94
PT	5+54.94	16.95
	5+60.00	16.96
	5+70.00	16.98
	5+80.00	17.02
POT	5+89.07	17.06

CURB RETURN STAKEOUT
LELAND - NORTH

POINT	STATION	ELEVATION
POC	10+00.00	16.67
	10+10.00	16.70
	10+20.00	16.74
	10+30.00	16.77
	10+40.00	16.81
PT	10+45.96	16.83
	10+50.00	16.84
	10+60.00	16.88
	10+70.00	16.91
	10+80.00	16.94
POT	10+81.59	16.95



COMBINATION CURB AND GUTTER
 (NOT TO SCALE)
 12 INCH GUTTER PAN, 10 INCH DEPTH, 8 INCH HEIGHT

- NOTES:**
- STA 103+22.95 CONSTR. S. CLINTON ST. = STA 1+00.00 CONSTR. LELAND AVE
 - FOR PEDESTRIAN RAMP DETAILS, SEE DE-03.

ADDENDUM NO. 1, PAGE 31 OF 73

DRAWN BY _____
 CHECKED BY _____

REVISIONS	
NO.	DESCRIPTION

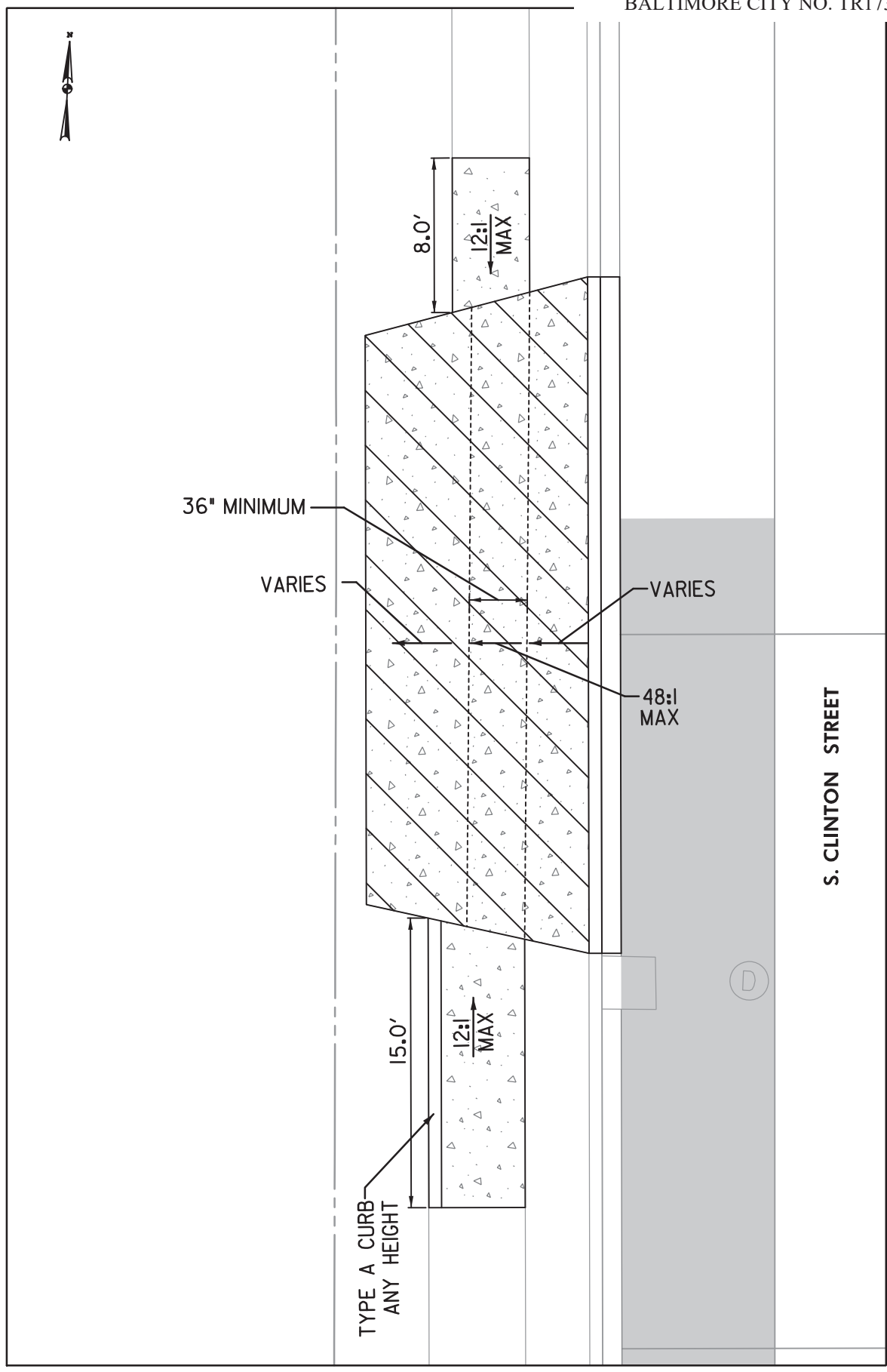
F.A.P. NO. AC-STBG-3241(5)E
 SHA NO. BC410024
 BALTIMORE CITY NO. TR17302

DE-23

CITY OF BALTIMORE
 DEPARTMENT OF TRANSPORTATION
 CONTRACT No. TR17302
 S. CLINTON STREET REHABILITATION
 FROM BOSTON STREET TO KEITH AVENUE
 DETAIL SHEET

SCALE: 1"=7.5'

DATE: JULY 2019
 SHEET OF



S. CLINTON STREET

STA. III+50, LT

DRAWN BY _____
 CHECKED BY _____

REVISIONS		
NO.	DESCRIPTION	DATE BY

F.A.P. NO. AC-STBG-3241(5)E
 SHA NO. BC410024
 BALTIMORE CITY NO. TR17302

DE-24

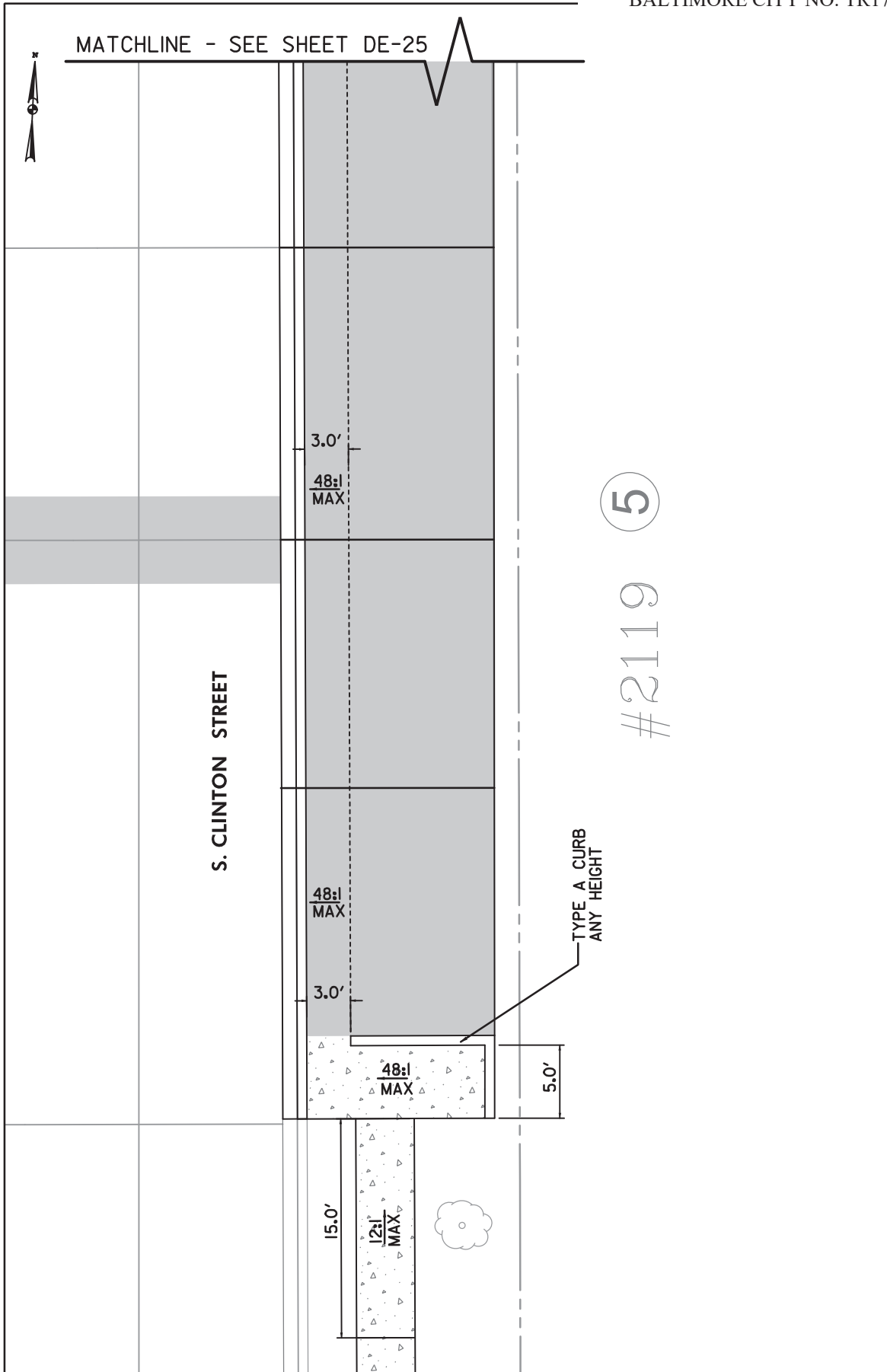
CITY OF BALTIMORE
 DEPARTMENT OF TRANSPORTATION
 CONTRACT No. TR17302
 S. CLINTON STREET REHABILITATION
 FROM BOSTON STREET TO KEITH AVENUE
 DETAIL SHEET

SCALE: 1"=10'

DATE: JULY 2019
 SHEET OF



STA. 112+00, RT



ADDENDUM NO. 1, PAGE 33 OF 73
 312R

DRAWN BY _____
 CHECKED BY _____

NO.	DESCRIPTION	DATE	BY

F.A.P. NO. AC-STBG-3241(5)E
 SHA NO. BC410024
 BALTIMORE CITY NO. TR17302

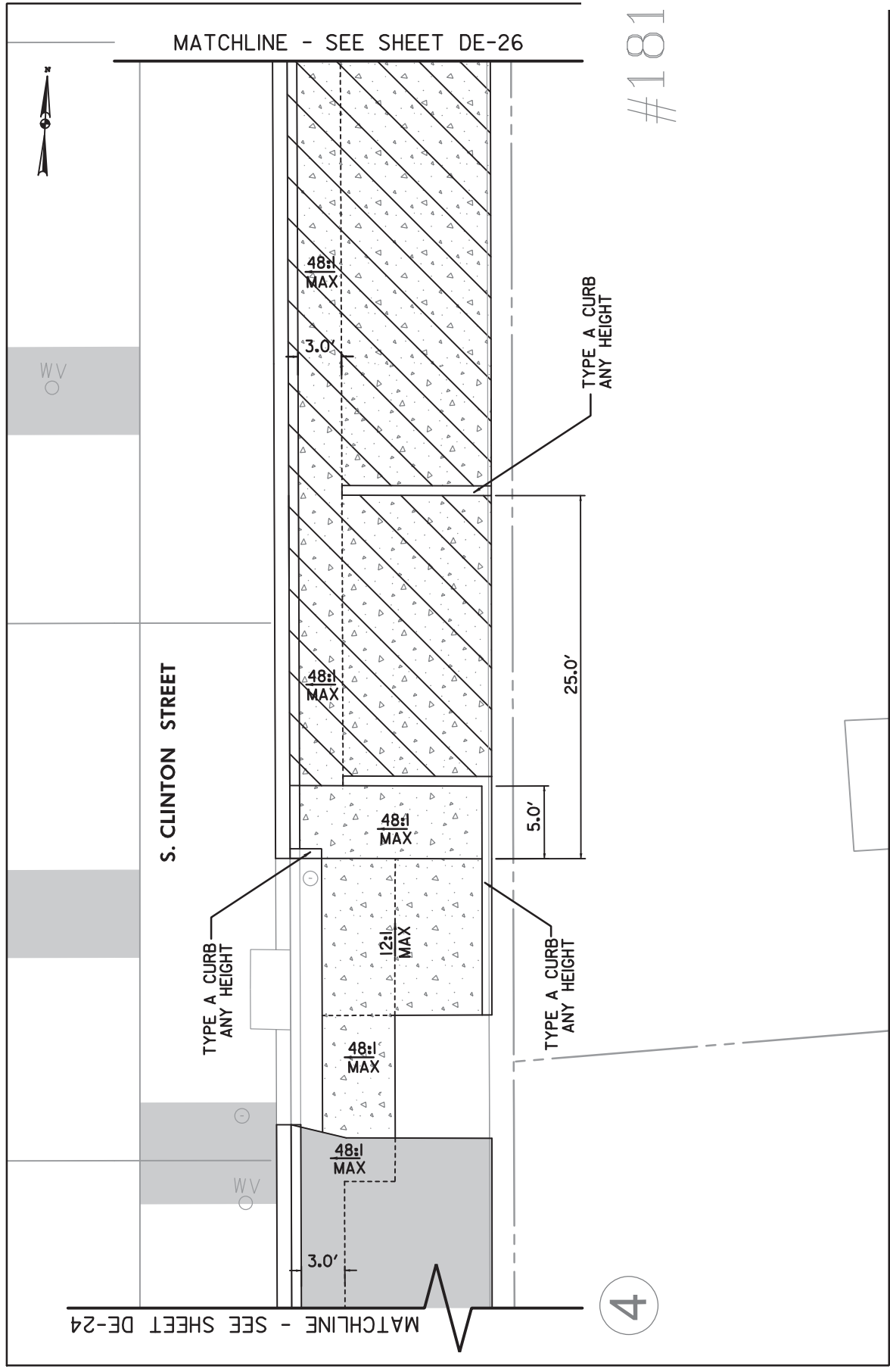
DE-25

CITY OF BALTIMORE
 DEPARTMENT OF TRANSPORTATION
 CONTRACT No. TR17302
 S. CLINTON STREET REHABILITATION
 FROM BOSTON STREET TO KEITH AVENUE
 DETAIL SHEET

SCALE: 1"=10'



DATE: JULY 2019
 SHEET OF



ADDENDUM NO. 1, PAGE 34 OF 73
 313R

DRAWN BY _____
 CHECKED BY _____

REVISIONS		
NO.	DESCRIPTION	DATE BY

F.A.P. NO. AC-STBG-3241(5)E
 SHA NO. BC410024
 BALTIMORE CITY NO. TR17302

DE-26

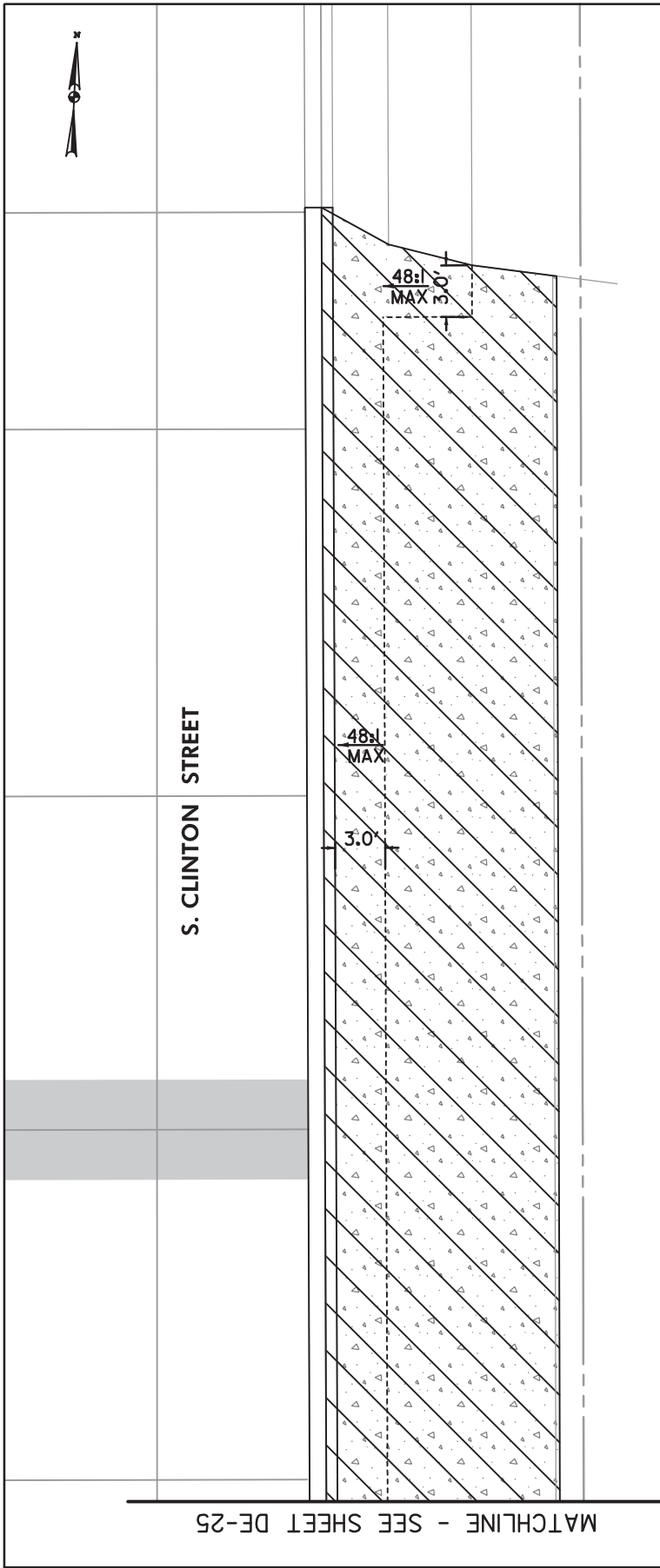
CITY OF BALTIMORE
 DEPARTMENT OF TRANSPORTATION
 CONTRACT No. TR17302
 S. CLINTON STREET REHABILITATION
 FROM BOSTON STREET TO KEITH AVENUE
 DETAIL SHEET

SCALE: 1"=10'

DATE: JULY 2019
 SHEET OF



STA. 114+50, RT



MATCHLINE - SEE SHEET DE-25

#1811 2

ADDENDUM NO. 1, PAGE 35 OF 73
 314R

DRAWN BY _____
 CHECKED BY _____

NO.	REVISIONS DESCRIPTION	DATE	BY

F.A.P. NO. AC-STBG-3241(5)E
 SHA NO. BC410024
 BALTIMORE CITY NO. TR17302

DE-27

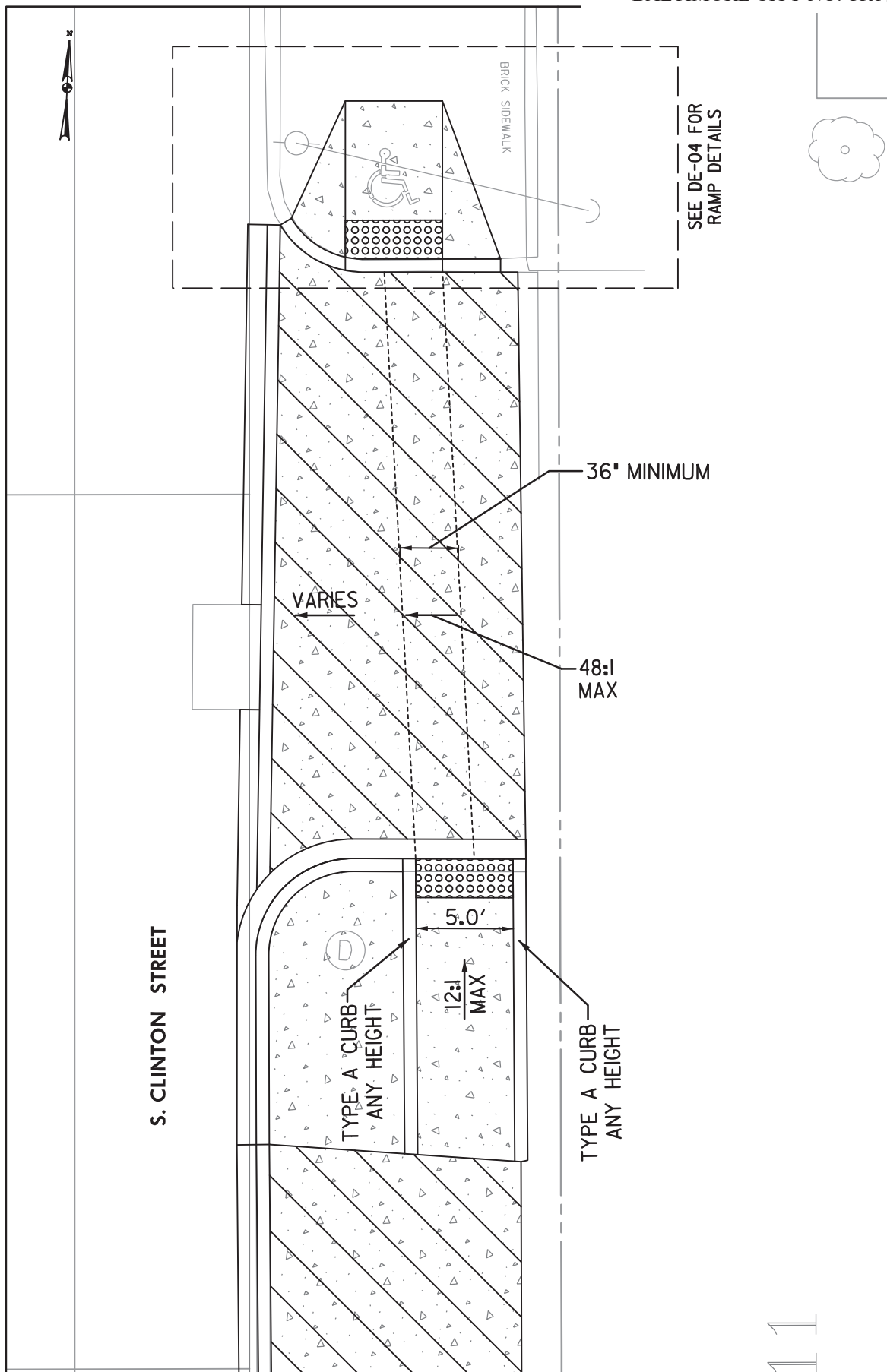
CITY OF BALTIMORE
 DEPARTMENT OF TRANSPORTATION
 CONTRACT No. TR17302
 S. CLINTON STREET REHABILITATION
 FROM BOSTON STREET TO KEITH AVENUE
 DETAIL SHEET

SCALE: 1"=7.5'

DATE: JULY 2019
 SHEET OF



STA. 116+40, RT



DRAWN BY _____
 CHECKED BY _____

REVISIONS	
NO.	DESCRIPTION

F.A.P. NO. AC-STBG-3241(5)E
 SHA NO. BC410024
 BALTIMORE CITY NO. TR17302

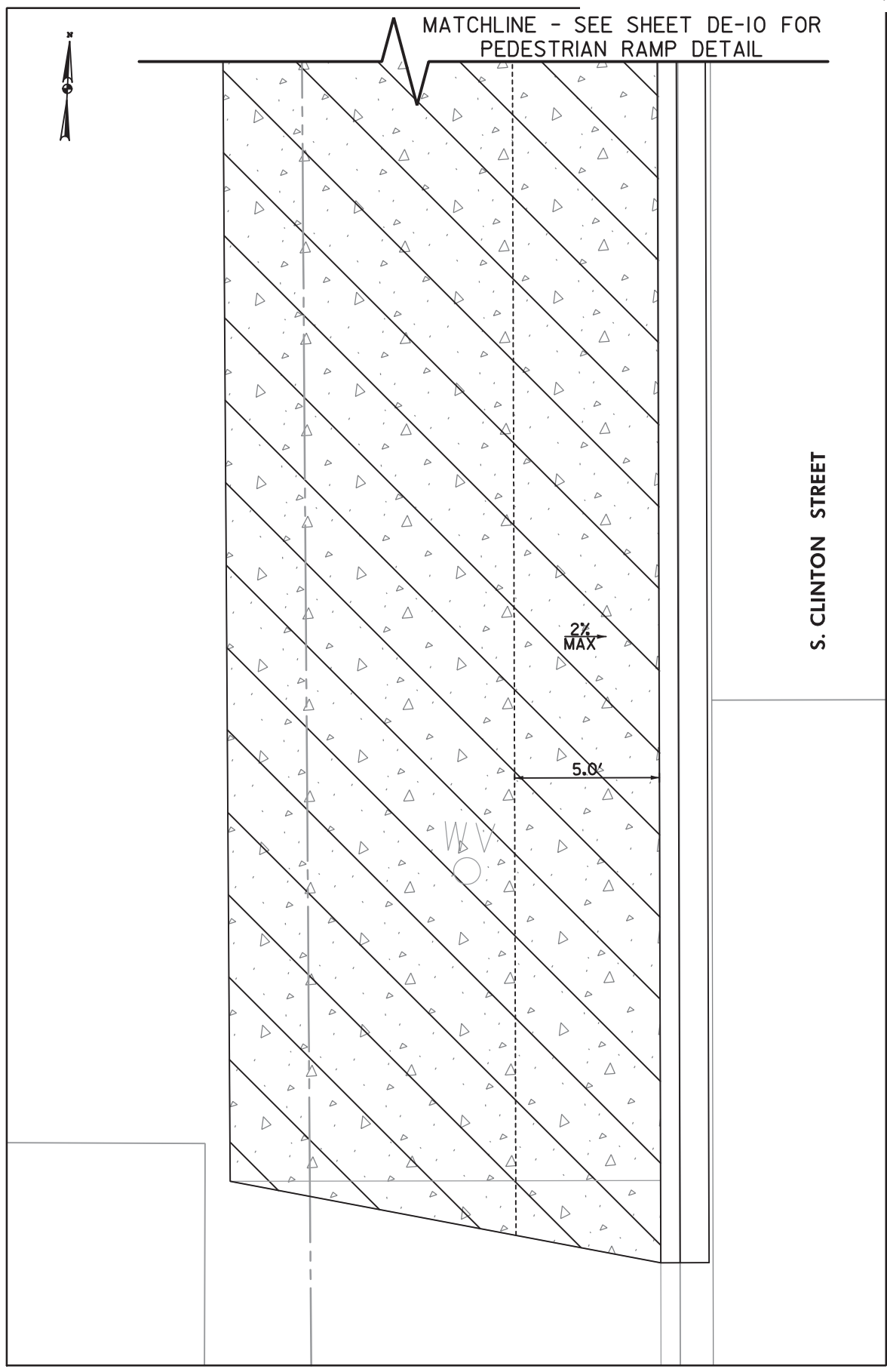
DE-28

CITY OF BALTIMORE
 DEPARTMENT OF TRANSPORTATION
 CONTRACT No. TR17302
 S. CLINTON STREET REHABILITATION
 FROM BOSTON STREET TO KEITH AVENUE
 DETAIL SHEET

SCALE: 1"=5'
 DATE: JULY 2019
 SHEET OF



MATCHLINE - SEE SHEET DE-10 FOR
 PEDESTRIAN RAMP DETAIL



DRAWN BY _____
 CHECKED BY _____

NO.	DESCRIPTION	DATE	BY

MATCH LINE STA. 138+50 - SEE SHEET PJ-02

F.A.P. NO. AC-STBG-3241(5)E
 SHA NO. BC410024
 BALTIMORE CITY NO. TR17302

PJ-01

CITY OF BALTIMORE
 DEPARTMENT OF TRANSPORTATION
 CONTRACT NO. TR17302
 S. CLINTON STREET REHABILITATION
 FROM BOSTON STREET TO KEITH AVENUE
 PAVEMENT JOINT LAYOUT PLAN

SCALE: 1"=20'

DATE: JULY 2019
 SHEET

TO KEITH AVENUE

TO BOSTON STREET



W 13125 | N -8775

W 13250 | N -8775

EXISTING ROW LINE

EXISTING ROW LINE

STA 137+54.00 EL EX.	EL 14.18	EL 14.46	EL 14.68	EL 14.91	EL 15.31
EL EX.	EL 14.43	EL 14.71	EL 14.95	EL 15.14	EL 15.56
N 2° 52' 28.13" W +50	+60.00 EL 14.78	+80.00 EL 15.02	+00.00 EL 15.28	+15.00 EL 15.46	+41.00 EL 15.83
STA 137+54.00 EL EX.	EL 15.13	EL 15.34	EL 15.61	EL 15.75	EL 16.10
	EL 14.94	EL 15.15	EL 15.38	EL 15.57	EL 15.89

NOTES:

- MODIFICATIONS TO THE JOINT LAYOUT WILL BE NECESSARY AT PROPOSED INLET LOCATIONS, SEE BC 380.99 FOR DETAILS OF THE PAVEMENT DEPRESSION LAYOUT AT EACH INLET.
- REFER TO DE-02 FOR ADDITIONAL REINFORCEMENT DETAILS AT MANHOLE STRUCTURES.

W 13250 | N -8900

ADDENDUM NO. 1, PAGE 38 OF 73
 317R

DRAWN BY _____
 CHECKED BY _____



NO.	DESCRIPTION	DATE	BY

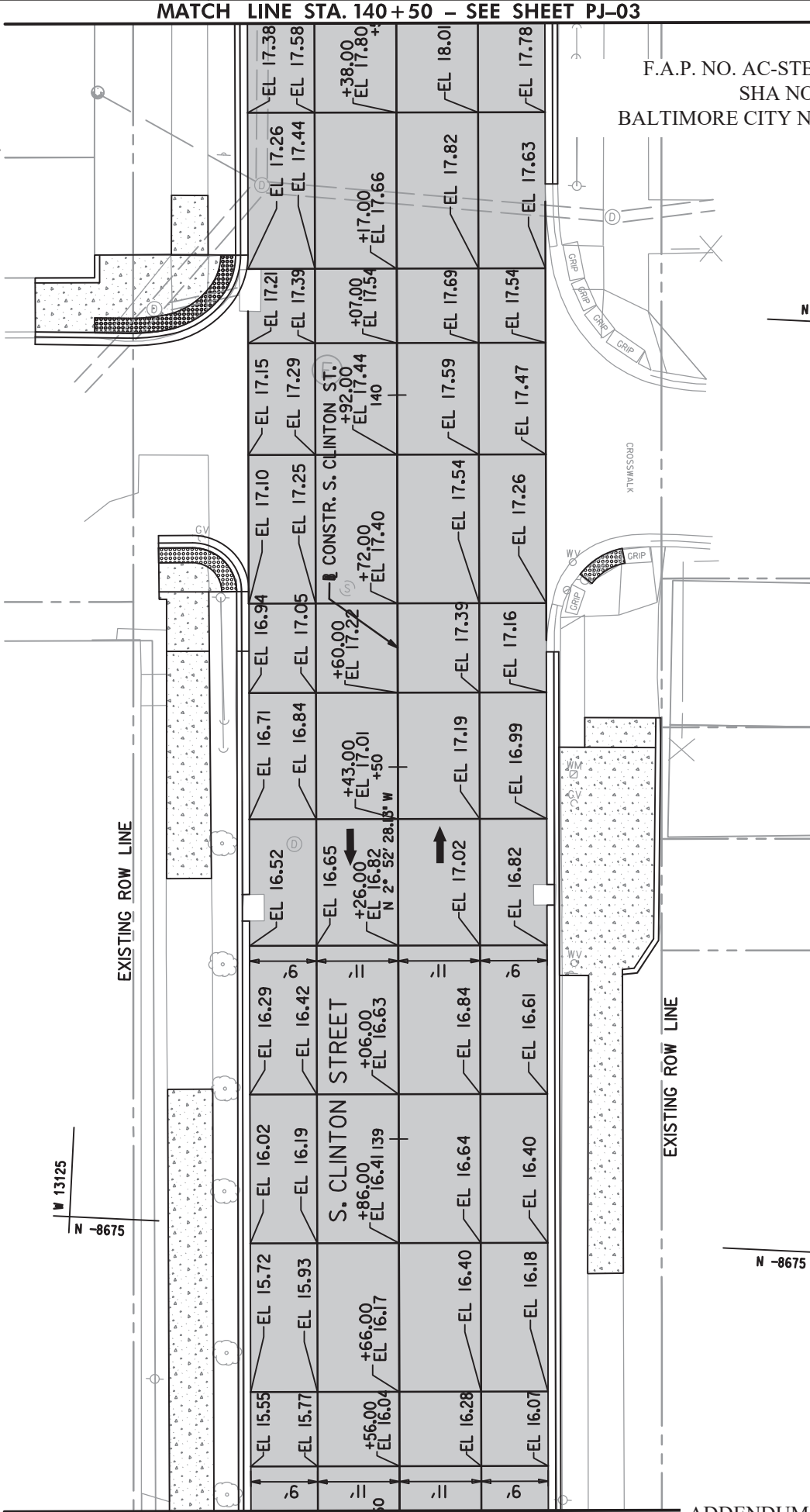


NO.	DESCRIPTION	DATE	BY

REVISIONS

TO KEITH AVENUE

TO BOSTON STREET



F.A.P. NO. AC-STBG-3241(5)E
SHA NO. BC410024
BALTIMORE CITY NO. TR17302

N -8550
W 13225

N -8675
W 13125

N -8675
W 13225

NOTES:

1. MODIFICATIONS TO THE JOINT LAYOUT WILL BE NECESSARY AT PROPOSED INLET LOCATIONS. SEE BC 380.99 FOR DETAILS OF THE PAVEMENT DEPRESSION LAYOUT AT EACH INLET.
2. REFER TO DE-02 FOR ADDITIONAL REINFORCEMENT DETAILS AT MANHOLE STRUCTURES.

PJ-02

CITY OF BALTIMORE
DEPARTMENT OF TRANSPORTATION
CONTRACT No. TR17302
S. CLINTON STREET REHABILITATION
FROM BOSTON STREET TO KEITH AVENUE
PAVEMENT JOINT LAYOUT PLAN

SCALE: 1"=20'

DATE: JULY 2019
SHEET



MATCH LINE STA. 138+50 - SEE SHEET PJ-01

ADDENDUM NO. 1, PAGE 39 OF 73
318R

DRAWN BY _____
CHECKED BY _____

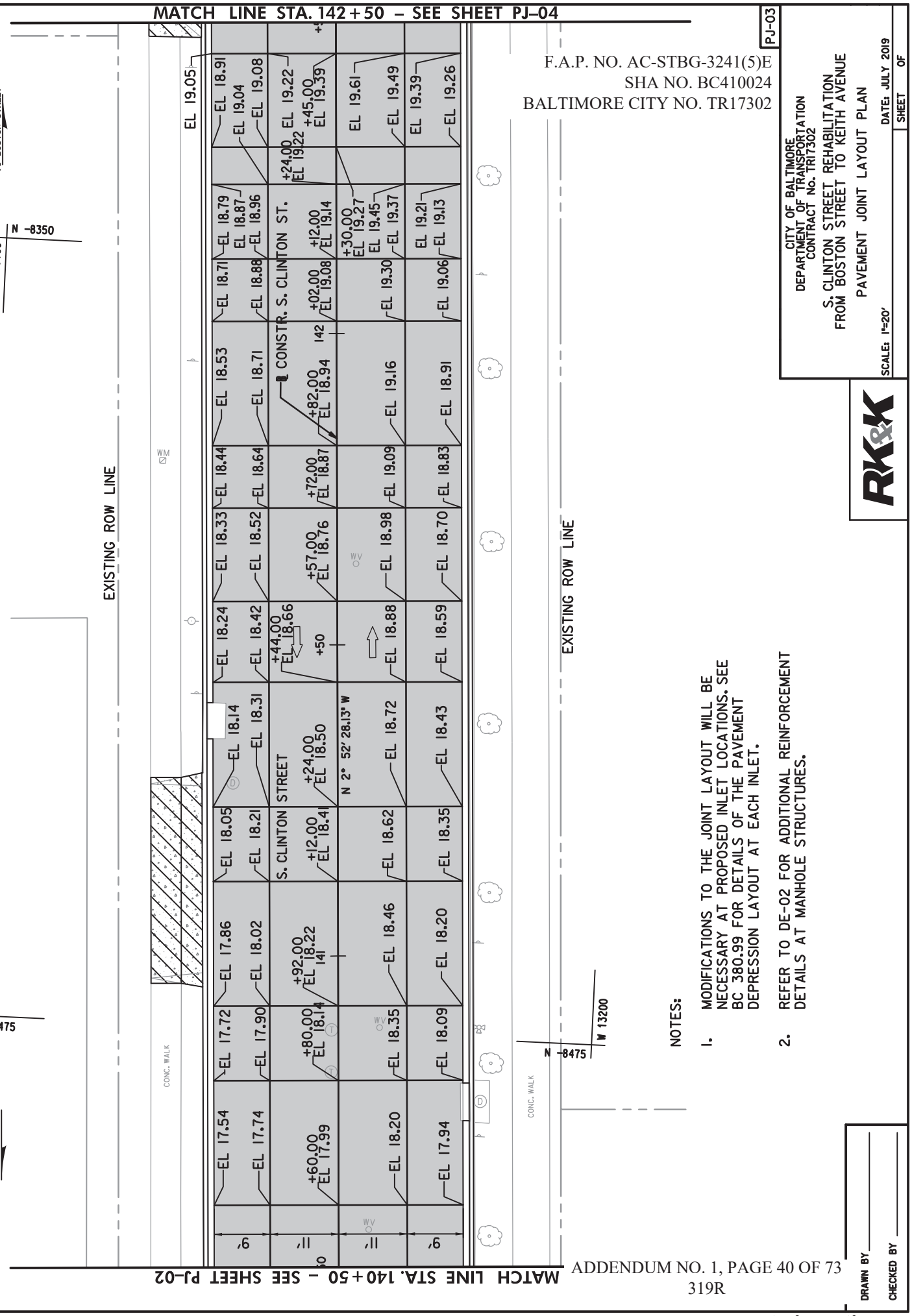
NO.	DESCRIPTION	DATE	BY



W 13100
N -8475

TO KEITH AVENUE

TO BOSTON STREET



NO.	DESCRIPTION	DATE	BY



W 13100
N -8350

EXISTING ROW LINE

EXISTING ROW LINE

MATCH LINE STA. 140+50 - SEE SHEET PJ-02

MATCH LINE STA. 142+50 - SEE SHEET PJ-04

ADDENDUM NO. 1, PAGE 40 OF 73
319R

NOTES:

- MODIFICATIONS TO THE JOINT LAYOUT WILL BE NECESSARY AT PROPOSED INLET LOCATIONS. SEE BC 380.99 FOR DETAILS OF THE PAVEMENT DEPRESSION LAYOUT AT EACH INLET.
- REFER TO DE-02 FOR ADDITIONAL REINFORCEMENT DETAILS AT MANHOLE STRUCTURES.

DRAWN BY _____
CHECKED BY _____



SCALE: 1"=20'

DATE: JULY 2019
SHEET _____ OF _____

F.A.P. NO. AC-STBG-3241(5)E
SHA NO. BC410024
BALTIMORE CITY NO. TR17302

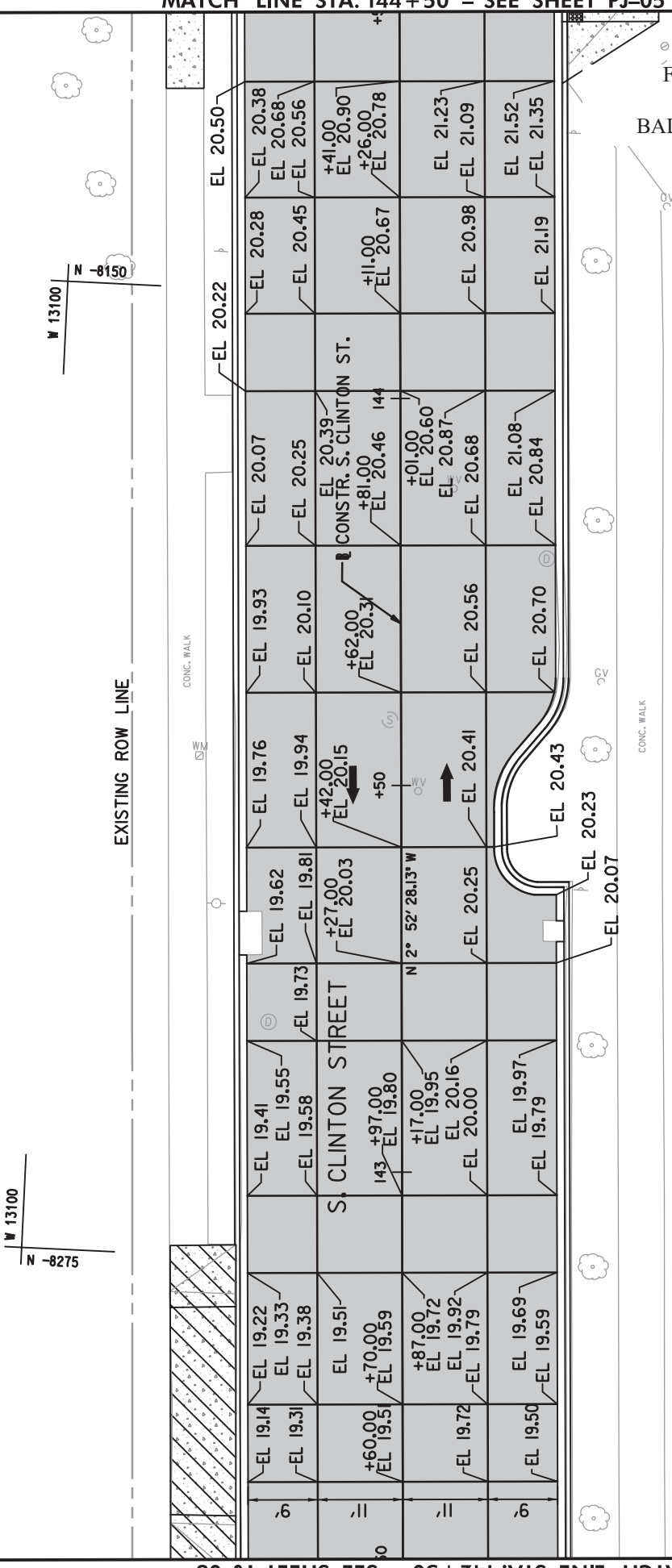
CITY OF BALTIMORE
DEPARTMENT OF TRANSPORTATION
CONTRACT No. TR17302
S. CLINTON STREET REHABILITATION
FROM BOSTON STREET TO KEITH AVENUE
PAVEMENT JOINT LAYOUT PLAN
PJ-03

NO.	DESCRIPTION	DATE	BY

MATCH LINE STA. 144 + 50 - SEE SHEET PJ-05

TO BOSTON STREET

TO KEITH AVENUE



F.A.P. NO. AC-STBG-3241(5)E
 SHA NO. BC410024
 BALTIMORE CITY NO. TR17302

PJ-04

CITY OF BALTIMORE
 DEPARTMENT OF TRANSPORTATION
 CONTRACT No. TR17302
 S. CLINTON STREET REHABILITATION
 FROM BOSTON STREET TO KEITH AVENUE
 PAVEMENT JOINT LAYOUT PLAN
 SCALE: 1"=20'
 DATE: JULY 2019
 SHEET OF



NOTES:

- MODIFICATIONS TO THE JOINT LAYOUT WILL BE NECESSARY AT PROPOSED INLET LOCATIONS, SEE BC 380.99 FOR DETAILS OF THE PAVEMENT DEPRESSION LAYOUT AT EACH INLET.
- REFER TO DE-02 FOR ADDITIONAL REINFORCEMENT DETAILS AT MANHOLE STRUCTURES.

MATCH LINE STA. 142 + 50 - SEE SHEET PJ-03

ADDENDUM NO. 1, PAGE 41 OF 73
 320R

DRAWN BY _____
 CHECKED BY _____

NO.	DESCRIPTION	DATE	BY



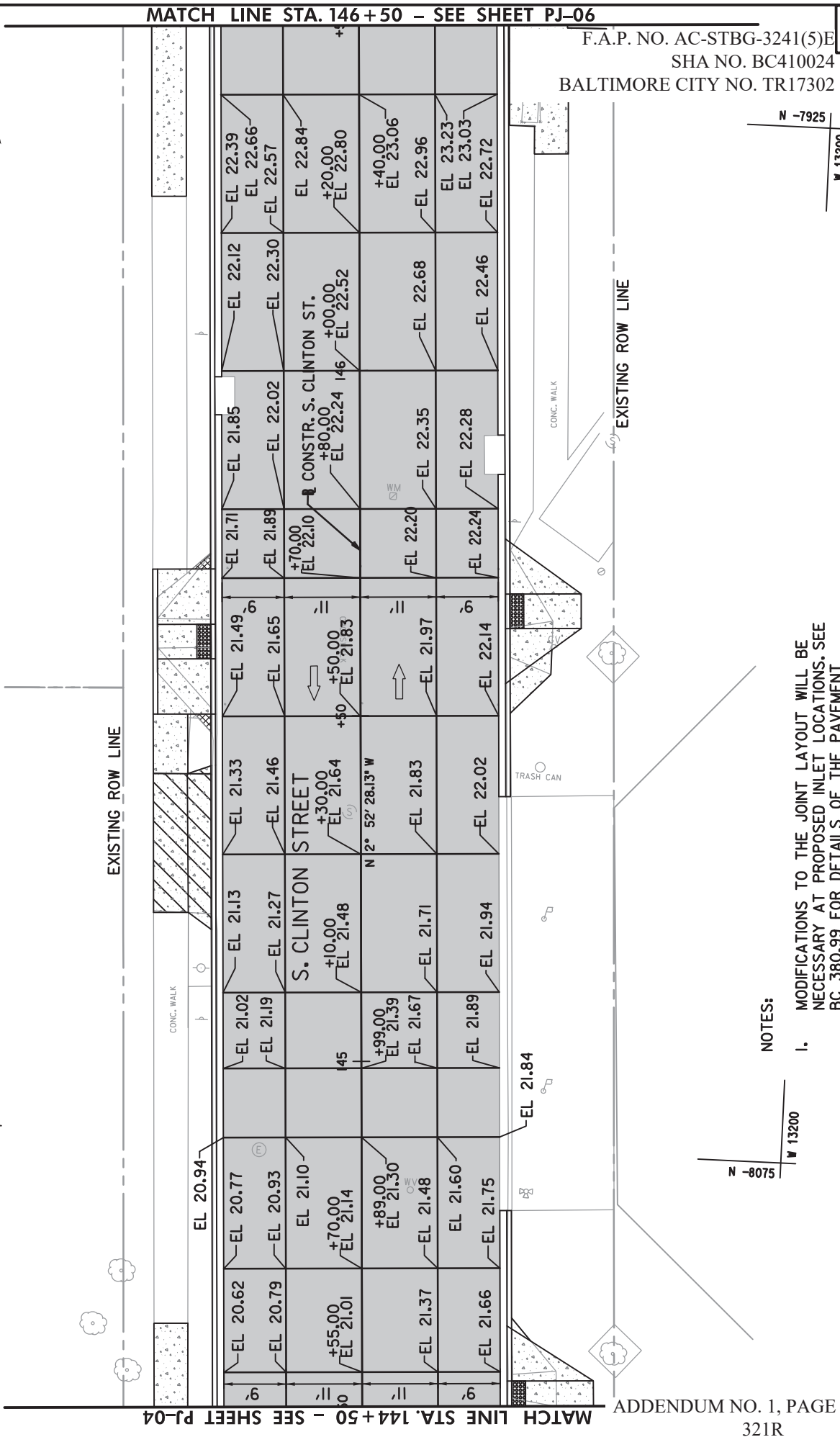
W 13075
N -8075

TO KEITH AVENUE

TO BOSTON STREET

MATCH LINE STA. 146+50 - SEE SHEET PJ-06

MATCH LINE STA. 144+50 - SEE SHEET PJ-04



F.A.P. NO. AC-STBG-3241(5)E
SHA NO. BC410024
BALTIMORE CITY NO. TR17302

PJ-05

CITY OF BALTIMORE
DEPARTMENT OF TRANSPORTATION
CONTRACT NO. TR17302
S. CLINTON STREET REHABILITATION
FROM BOSTON STREET TO KEITH AVENUE
PAVEMENT JOINT LAYOUT PLAN
SCALE: 1"=20'
DATE: JULY 2019
SHEET OF



NOTES:

1. MODIFICATIONS TO THE JOINT LAYOUT WILL BE NECESSARY AT PROPOSED INLET LOCATIONS. SEE BC 380.99 FOR DETAILS OF THE PAVEMENT DEPRESSION LAYOUT AT EACH INLET.
2. REFER TO DE-02 FOR ADDITIONAL REINFORCEMENT DETAILS AT MANHOLE STRUCTURES.

N -8075
W 13200

ADDENDUM NO. 1, PAGE 42 OF 73
321R

DRAWN BY _____
CHECKED BY _____

NO.	DESCRIPTION	DATE	BY



W 13075
N -7875

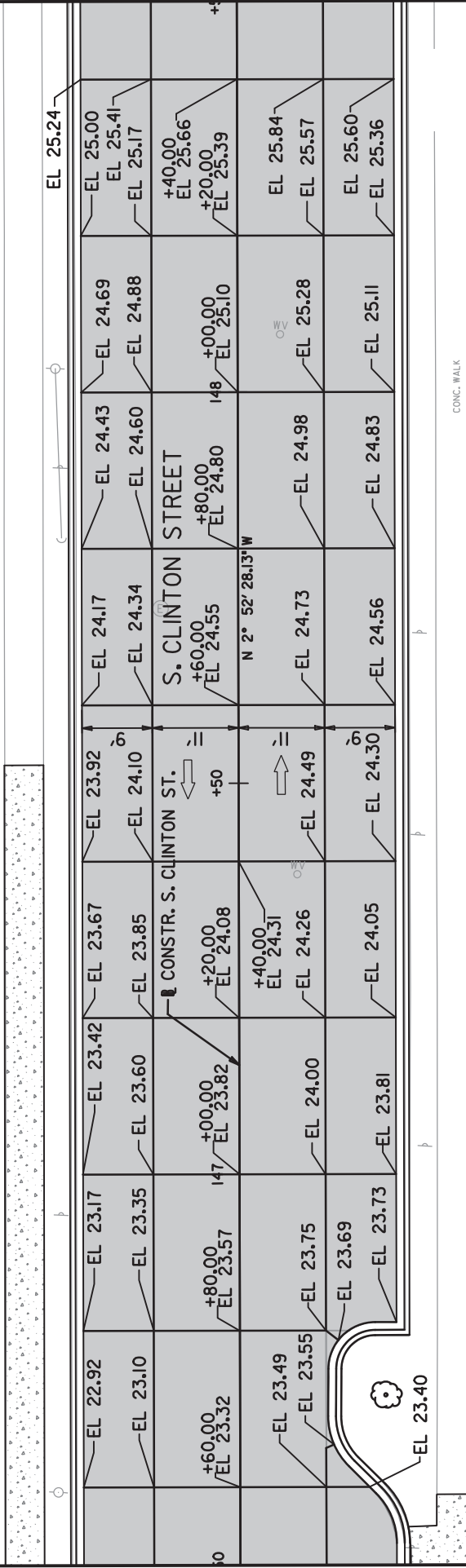
TO KEITH AVENUE

TO BOSTON STREET

W 13075
N -7750

EXISTING ROW LINE

EXISTING ROW LINE



F.A.P. NO. AC-STBG-3241(5)E
SHA NO. BC410024
BALTIMORE CITY NO. TR17302

NOTES:

- MODIFICATIONS TO THE JOINT LAYOUT WILL BE NECESSARY AT PROPOSED INLET LOCATIONS. SEE BC 380.99 FOR DETAILS OF THE PAVEMENT DEPRESSION LAYOUT AT EACH INLET.
- REFER TO DE-02 FOR ADDITIONAL REINFORCEMENT DETAILS AT MANHOLE STRUCTURES.

W 13200
N -7875

PJ-06

CITY OF BALTIMORE
DEPARTMENT OF TRANSPORTATION
CONTRACT No. TR17302
S. CLINTON STREET REHABILITATION
FROM BOSTON STREET TO KEITH AVENUE
PAVEMENT JOINT LAYOUT PLAN

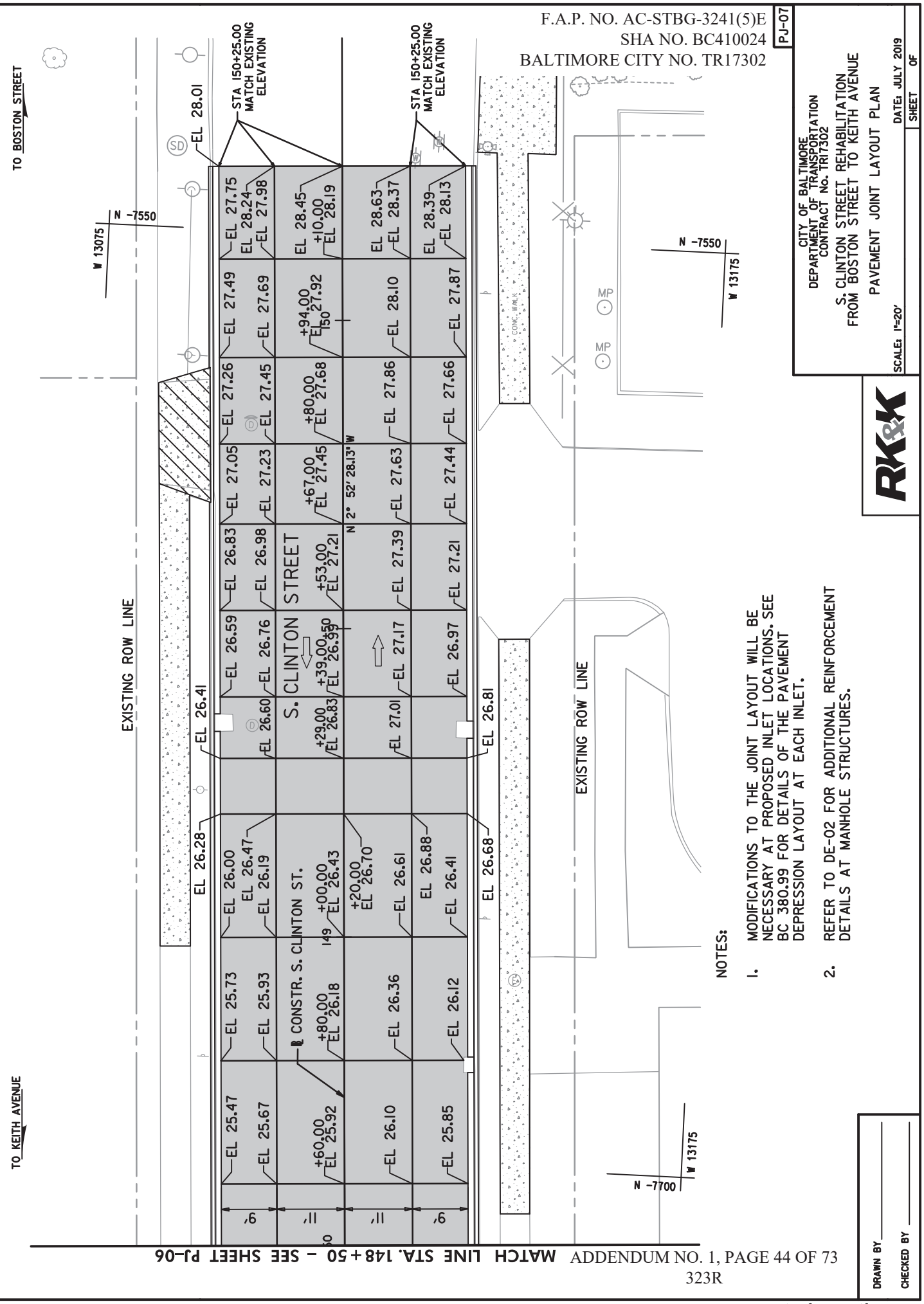


SCALE: 1"=20'

DATE: JULY 2019
SHEET

DRAWN BY _____
CHECKED BY _____

NO.	DESCRIPTION	DATE	BY



F.A.P. NO. AC-STBG-3241(5)E
 SHA NO. BC410024
 BALTIMORE CITY NO. TR17302

PJ-07

CITY OF BALTIMORE
 DEPARTMENT OF TRANSPORTATION
 CONTRACT NO. TR17302

S. CLINTON STREET REHABILITATION
 FROM BOSTON STREET TO KEITH AVENUE

PAVEMENT JOINT LAYOUT PLAN

DATE: JULY 2019
 SCALE: 1"=20'
 SHEET OF



MATCH LINE STA. 148+50 - SEE SHEET PJ-06 ADDENDUM NO. 1, PAGE 44 OF 73 323R

- NOTES:
- MODIFICATIONS TO THE JOINT LAYOUT WILL BE NECESSARY AT PROPOSED INLET LOCATIONS. SEE BC 380.99 FOR DETAILS OF THE PAVEMENT DEPRESSION LAYOUT AT EACH INLET.
 - REFER TO DE-02 FOR ADDITIONAL REINFORCEMENT DETAILS AT MANHOLE STRUCTURES.

DRAWN BY _____
 CHECKED BY _____

F.A.P. NO. AC-STBG-3241(5)E
 SHA NO. BC410024
 BALTIMORE CITY NO. TR17302

PS-01

NO.	DESCRIPTION	DATE	BY

TO BOSTON STREET



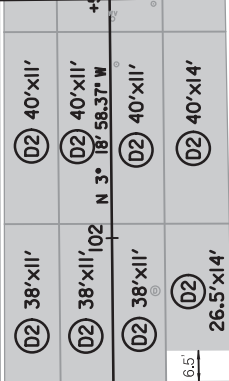
N 13250
 -12375

BASELINE CONTROL COORDINATES			
CURVE	STATION	NORTH	EAST
BASELINE OF S. CLINTON ST.			
POT	100+00.00	-12,557.5008	13,364.8043

EXISTING ROW LINE

#2272 (2)

±5.5'



EXISTING ROW LINE #000C (13)

RUKERT TERMINAL CORP. RAILROAD TRAIL

N 13450
 -12375

CITY OF BALTIMORE
 DEPARTMENT OF TRANSPORTATION
 CONTRACT NO. TR17302
 S. CLINTON STREET REHABILITATION
 FROM BOSTON STREET TO KEITH AVENUE
 ROADWAY PLAN

SCALE: 1"=40'

DATE: JULY 2019
 SHEET OF

- CONCRETE REPAIR METHOD:**
- (A) STD. NO. MD 577.02 METHOD A, TYPE 1
 - (B) STD. NO. MD 577.03 METHOD B, TYPE 1
 - (B2) STD. NO. MD 577.03 METHOD B, TYPE 2
 - (C) STD. NO. MD 577.04 METHOD C, TYPE 1
 - (C2) STD. NO. MD 577.04 METHOD C, TYPE 2
 - (D2) STD. NO. MD 577.05 METHOD D, TYPE 2
 - (E) STD. NO. MD 577.06 METHOD E, TYPE 1
 - (E2) STD. NO. MD 577.06 METHOD E, TYPE 2

LIMIT OF WORK
 TR17302
 S. CLINTON ST
 STA. 101 + 04

S. CLINTON STREET

CONSTR. S. CLINTON ST.

GENERAL PAVEMENT REPAIR NOTES:

1. JOINT SEALS IN POOR CONDITION SHALL BE CLEANED AND SEALED IN ACCORDANCE WITH SECTIONS 510 AND 523 OF THE SPECIFICATIONS.
2. LINEAR CRACKS 1/8" WIDE AND WIDER SHALL BE SEALED IN ACCORDANCE WITH SECTION 510 OF THE SPECIFICATIONS.
3. CONCRETE REPAIRS SHALL BE PERFORMED IN ACCORDANCE WITH SECTION 522 OF THE SPECIFICATIONS.
4. TRANSVERSE JOINT SPACING FOR NEW SLABS SHALL NOT EXCEED 20-FT, AND LINE UP WITH EXISTING JOINTS IN THE CURB & GUTTER.
5. FOR TYPE 1 & 2 REPAIRS ADJACENT TO CURB THAT IS NOT TO BE MODIFIED, REFER TO PAVEMENT DETAIL A ON DE-1.
6. REFER TO DE-02 FOR ADDITIONAL REINFORCING AROUND MANHOLE STRUCTURES. COST OF ADDITIONAL REINFORCEMENT SHALL BE INCIDENTAL TO PERTINENT CONCRETE REPAIRS.
7. CONCRETE REPAIRS, SIDEWALK REPLACEMENT AND CURB REPLACEMENT SHALL END 8 FEET FROM THE CENTERLINE OF ALL RAILROAD TRACKS.
8. NOTIFY "MISS UTILITY" 3 WORKING DAYS PRIOR TO EXCAVATION OR SIMILAR WORK IS TO BE PERFORMED.
9. MAINTAIN 10 FOOT CLEARANCE FROM OVERHEAD ELECTRIC LINES TO COMPLY WITH THE MARYLAND HIGH VOLTAGE LINE ACT.
10. DRIVEWAY APRON REPLACEMENT SHALL TIE IN TO EXISTING JOINT.

LEGEND

- PAVEMENT REMOVAL
- DETECTABLE WARNING SURFACE (BC 655.40)
- CONCRETE SIDEWALK REPLACEMENT
- CONCRETE DRIVEWAY APRON REPLACEMENT
- EXISTING PAVEMENT JOINTS
- 100' CRITICAL AREA BUFFER
- FULL SLAB REPLACEMENT
- CONCRETE REPAIR
- EXISTING TREE
- PROPOSED TREE



TO KEITH AVENUE

N 13250
 -12675

DRAWN BY _____
 CHECKED BY _____

\\bdr\vol1\2014\2014\1518\BC1191\Task 6 - Clinton Street\CADD\Plans\PHD-POOL-Clinton-St.dgn Thursday, February 13, 2020 AT 04:49 PM

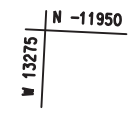
NO.	DESCRIPTION	DATE	BY

MATCH LINE STA. 106+50 - SEE SHEET PS-03

F.A.P. NO. AC-STBG-3241(5)E
SHA NO. BC410024
BALTIMORE CITY NO. TR17302

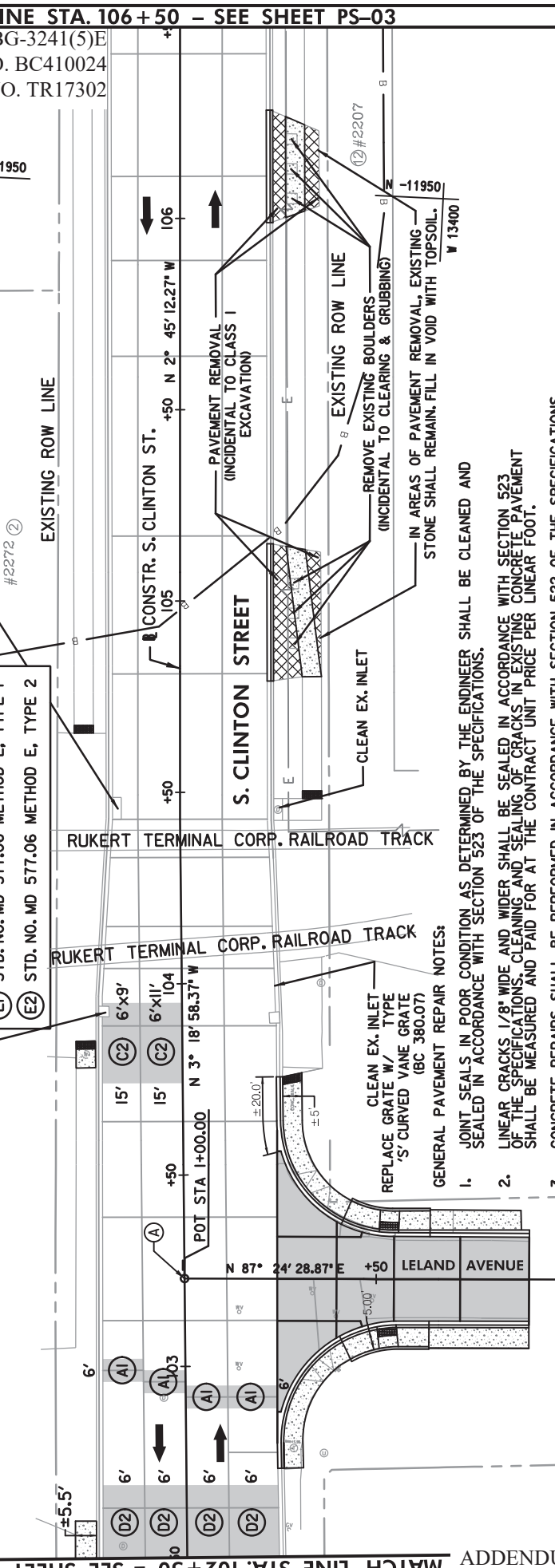
TO BOSTON STREET

TO KEITH AVENUE



CLEAN EX. INLET
REPLACE GRATE W/
'E' CURVED VANE GRATES
(BC 376.03)

CLEAN EX. INLET
REPLACE GRATE W/
'S' CURVED VANE GRATE
(BC 380.07)



CONCRETE REPAIR METHOD:

(A1)	STD. NO. MD 577.02 METHOD A, TYPE 1
(B1)	STD. NO. MD 577.03 METHOD B, TYPE 1
(B2)	STD. NO. MD 577.03 METHOD B, TYPE 2
(C1)	STD. NO. MD 577.04 METHOD C, TYPE 1
(C2)	STD. NO. MD 577.04 METHOD C, TYPE 2
(D2)	STD. NO. MD 577.05 METHOD D, TYPE 2
(E1)	STD. NO. MD 577.06 METHOD E, TYPE 1
(E2)	STD. NO. MD 577.06 METHOD E, TYPE 2

- GENERAL PAVEMENT REPAIR NOTES:
1. JOINT SEALS IN POOR CONDITION AS DETERMINED BY THE ENGINEER SHALL BE CLEANED AND SEALED IN ACCORDANCE WITH SECTION 523 OF THE SPECIFICATIONS.
 2. LINEAR CRACKS 1/8" WIDE AND WIDER SHALL BE SEALED IN ACCORDANCE WITH SECTION 523 OF THE SPECIFICATIONS. CLEANING AND SEALING OF CRACKS IN EXISTING CONCRETE PAVEMENT SHALL BE MEASURED AND PAID FOR AT THE CONTRACT UNIT PRICE PER LINEAR FOOT.
 3. CONCRETE REPAIRS SHALL BE PERFORMED IN ACCORDANCE WITH SECTION 522 OF THE SPECIFICATIONS.
 4. TRANSVERSE JOINT SPACING FOR NEW SLABS SHALL NOT EXCEED 20'-FT., AND LINE UP WITH EXISTING JOINTS IN THE CURB & GUTTER.
 5. FOR TYPE 1 & 2 REPAIRS ADJACENT TO CURB THAT IS NOT TO BE MODIFIED, REFER TO PAVEMENT DETAIL A ON DE-1.
 6. REFER TO DE-02 FOR ADDITIONAL REINFORCING AROUND MANHOLE STRUCTURES. COST OF ADDITIONAL REINFORCEMENT SHALL BE INCIDENTAL TO PERTINENT CONCRETE REPAIRS.
 7. CONCRETE REPAIRS, SIDEWALK REPLACEMENT AND CURB REPLACEMENT SHALL END 8 FEET FROM THE CENTERLINE OF ALL RAILROAD TRACKS.
 8. NOTIFY "MISS UTILITY" 3 WORKING DAYS PRIOR TO EXCAVATION OR SIMILAR WORK IS TO BE PERFORMED.
 9. MAINTAIN 10 FOOT CLEARANCE FROM OVERHEAD ELECTRIC LINES TO COMPLY WITH THE MARYLAND HIGH VOLTAGE LINE ACT.
 10. DRIVEWAY APRON REPLACEMENT SHALL TIE IN TO EXISTING JOINT.

BASELINE CONTROL COORDINATES		
CURVE	STATION	NORTH
EAST		
BASELINE OF S. CLINTON ST.		
POT	104+43.00	-12,115.2452
		13,339.1785

STATION EQUATIONS

- (A) STA 103+22.95 CONSTR. S. CLINTON ST. = STA 1+00.00 CONSTR. LELAND AVE
- FOR INTERSECTION DETAILS, SEE DE-22.

PS-02

CITY OF BALTIMORE
DEPARTMENT OF TRANSPORTATION
CONTRACT NO. TR17302

S. CLINTON STREET REHABILITATION
FROM BOSTON STREET TO KEITH AVENUE
ROADWAY PLAN

DATE: JULY 2019
SHEET 0F

LEGEND

	PAVEMENT REMOVAL		DETECTABLE WARNING SURFACE (BC 655.40)
	FULL SLAB REPLACEMENT		CONCRETE REPAIR
	EXISTING TREE		PROPOSED TREE
	CONCRETE SIDEWALK REPLACEMENT		EXISTING PAVEMENT JOINTS
	CONCRETE DRIVEWAY APRON REPLACEMENT		100' CRITICAL AREA BUFFER



MATCH LINE STA. 102+50 - SEE SHEET PS-01

ADDENDUM NO. 1, PAGE 46 OF 73
325R

DRAWN BY _____
CHECKED BY _____

NO.	DESCRIPTION	DATE	BY

REVISIONS

DATE

BY

NO. DESCRIPTION DATE BY

TREE BRANCH PRUNING 7 FEET ABOVE GROUND LEVEL
T1
T2
T3
T4
T5
T6
T7
T8
T9
T10
T11

CONCRETE REPAIR METHOD:
(A1) STD. NO. MD 577.02 METHOD A, TYPE 1
(B1) STD. NO. MD 577.03 METHOD B, TYPE 1
(B2) STD. NO. MD 577.03 METHOD B, TYPE 2
(C1) STD. NO. MD 577.04 METHOD C, TYPE 1
(C2) STD. NO. MD 577.04 METHOD C, TYPE 2
(D2) STD. NO. MD 577.05 METHOD D, TYPE 2
(E1) STD. NO. MD 577.06 METHOD E, TYPE 1
(E2) STD. NO. MD 577.06 METHOD E, TYPE 2

CLEAN EX. INLET
REPLACE GRATES W/ TYPE
'S' CURVED VANE GRATES
(BC 380.07)

CLEAN EX. INLET
REPLACE GRATES W/ TYPE
'S' CURVED VANE GRATE
(BC 380.07)

MATCH LINE STA. 110+50 - SEE SHEET PS-04

MATCH LINE STA. 106+50 - SEE SHEET PS-02

F.A.P. NO. AC-STBG-3241(5)E
SHA NO. BC410024
BALTIMORE CITY NO. TR17302

PS-03

CITY OF BALTIMORE
DEPARTMENT OF TRANSPORTATION
CONTRACT NO. TR17302
S. CLINTON STREET REHABILITATION
FROM BOSTON STREET TO KEITH AVENUE
ROADWAY PLAN

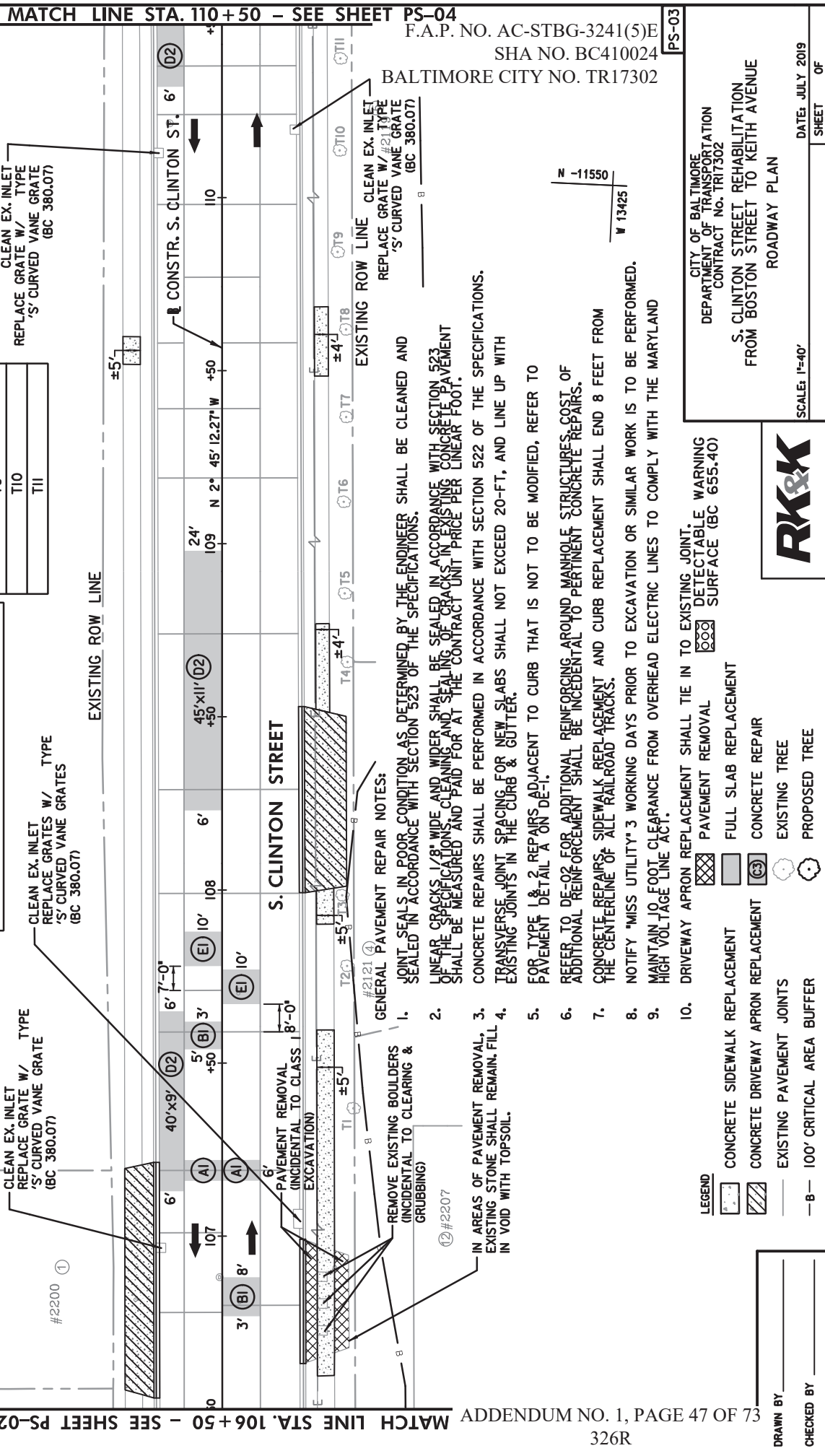
DATE: JULY 2019
SHEET OF

SCALE: 1"=40'



TO KEITH AVENUE
W 13225
N -11850

TO BOSTON STREET
W 13225
N -11550



GENERAL PAVEMENT REPAIR NOTES:

1. JOINT SEALS IN POOR CONDITION AS DETERMINED BY THE ENGINEER SHALL BE CLEANED AND SEALED IN ACCORDANCE WITH SECTION 523 OF THE SPECIFICATIONS.
2. LINEAR CRACKS 1/8" WIDE AND WIDER SHALL BE SEALED IN ACCORDANCE WITH SECTION 523 OF THE SPECIFICATIONS. CLEANING AND SEALING OF CRACKS IN EXISTING CONCRETE PAVEMENT SHALL BE MEASURED AND PAID FOR AT THE CONTRACT UNIT PRICE PER LINEAR FOOT.
3. CONCRETE REPAIRS SHALL BE PERFORMED IN ACCORDANCE WITH SECTION 522 OF THE SPECIFICATIONS.
4. TRANSVERSE JOINT SPACING FOR NEW SLABS SHALL NOT EXCEED 20-FT, AND LINE UP WITH EXISTING JOINTS IN THE CURB & GUTTER.
5. FOR TYPE 1 & 2 REPAIRS ADJACENT TO CURB THAT IS NOT TO BE MODIFIED, REFER TO PAVEMENT DETAIL A ON DE-1.
6. REFER TO DE-02 FOR ADDITIONAL REINFORCING AROUND MANHOLE STRUCTURES. COST OF ADDITIONAL REINFORCEMENT SHALL BE INCIDENTAL TO PERTINENT CONCRETE REPAIRS.
7. CONCRETE REPAIRS, SIDEWALK REPLACEMENT AND CURB REPLACEMENT SHALL END 8 FEET FROM THE CENTERLINE OF ALL RAILROAD TRACKS.
8. NOTIFY 'MISS UTILITY' 3 WORKING DAYS PRIOR TO EXCAVATION OR SIMILAR WORK IS TO BE PERFORMED.
9. MAINTAIN 10 FOOT CLEARANCE FROM OVERHEAD ELECTRIC LINES TO COMPLY WITH THE MARYLAND HIGH VOLTAGE LINE ACT.
10. DRIVEWAY APRON REPLACEMENT SHALL TIE IN TO EXISTING JOINT.

LEGEND	DESCRIPTION
	PAVEMENT REMOVAL
	DETECTABLE WARNING SURFACE (BC 655.40)
	FULL SLAB REPLACEMENT
	CONCRETE DRIVEWAY APRON REPLACEMENT
	CONCRETE REPAIR
	EXISTING TREE
	PROPOSED TREE
	EXISTING PAVEMENT JOINTS
	100' CRITICAL AREA BUFFER

DRAWN BY _____
CHECKED BY _____

ADDENDUM NO. 1, PAGE 47 OF 73
326R

NO.	DESCRIPTION	DATE	BY

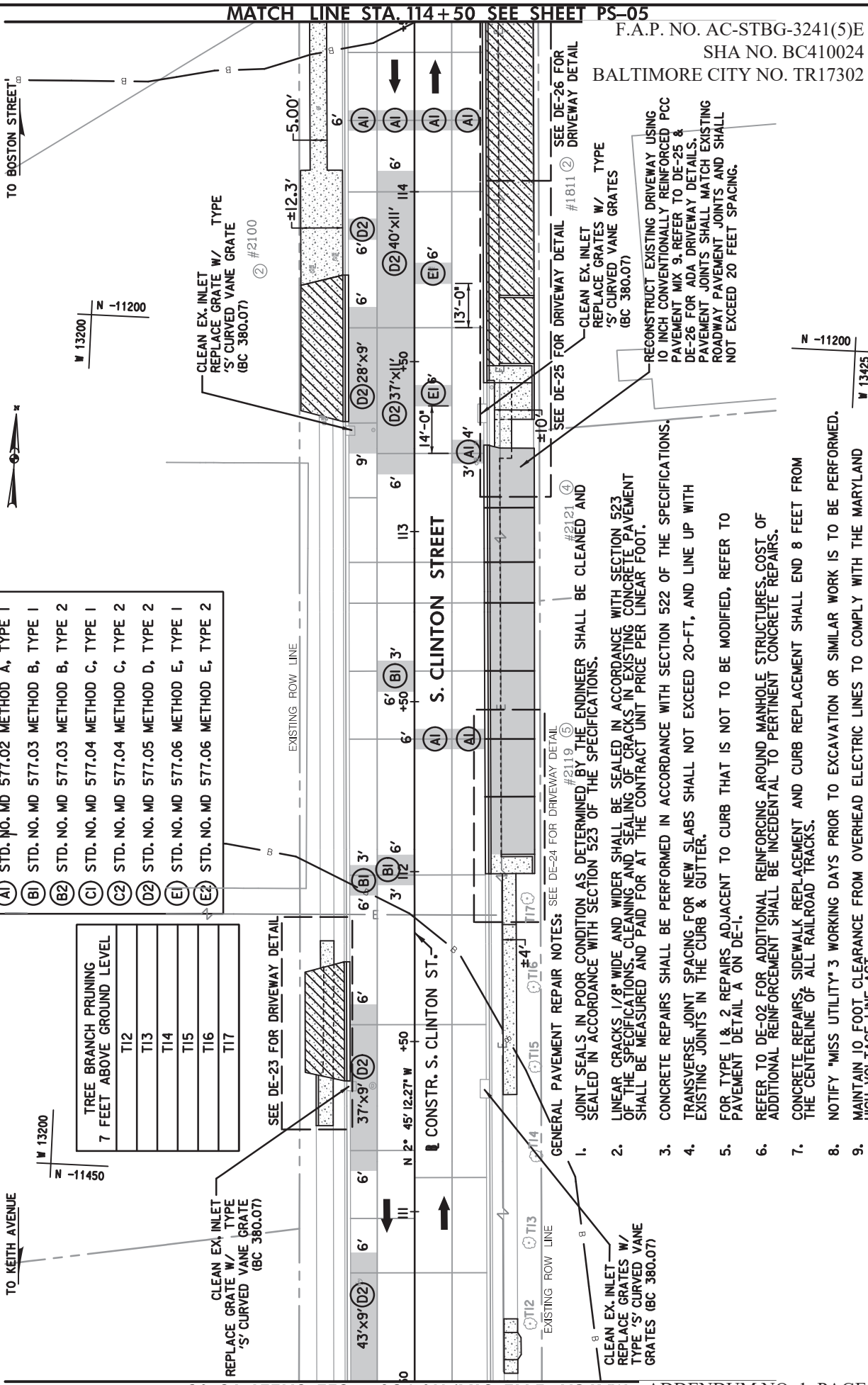
MATCH LINE STA. 114+50 SEE SHEET PS-05

F.A.P. NO. AC-STBG-3241(5)E
SHA NO. BC410024
BALTIMORE CITY NO. TR17302

PS-04

CITY OF BALTIMORE
DEPARTMENT OF TRANSPORTATION
CONTRACT NO. TR17302
S. CLINTON STREET REHABILITATION
FROM BOSTON STREET TO KEITH AVENUE
ROADWAY PLAN

DATE: JULY 2019
SHEET OF



CONCRETE REPAIR METHOD:

(A1)	STD. NO. MD 577.02 METHOD A, TYPE 1
(B1)	STD. NO. MD 577.03 METHOD B, TYPE 1
(B2)	STD. NO. MD 577.03 METHOD B, TYPE 2
(C1)	STD. NO. MD 577.04 METHOD C, TYPE 1
(C2)	STD. NO. MD 577.04 METHOD C, TYPE 2
(D2)	STD. NO. MD 577.05 METHOD D, TYPE 2
(E1)	STD. NO. MD 577.06 METHOD E, TYPE 1
(E2)	STD. NO. MD 577.06 METHOD E, TYPE 2

TREE BRANCH PRUNING
7 FEET ABOVE GROUND LEVEL

T12
T13
T14
T15
T16
T17

- GENERAL PAVEMENT REPAIR NOTES: SEE DE-24 FOR DRIVEWAY DETAIL #2119 (5)
- JOINT SEALS IN POOR CONDITION AS DETERMINED BY THE ENGINEER SHALL BE CLEANED AND SEALED IN ACCORDANCE WITH SECTION 523 OF THE SPECIFICATIONS.
 - LINEAR CRACKS 1/8" WIDE AND WIDER SHALL BE SEALED IN ACCORDANCE WITH SECTION 523 OF THE SPECIFICATIONS. CLEANING AND SEALING OF CRACKS IN EXISTING CONCRETE PAVEMENT SHALL BE MEASURED AND PAID FOR AT THE CONTRACT UNIT PRICE PER LINEAR FOOT.
 - CONCRETE REPAIRS SHALL BE PERFORMED IN ACCORDANCE WITH SECTION 522 OF THE SPECIFICATIONS.
 - TRANSVERSE JOINT SPACING FOR NEW SLABS SHALL NOT EXCEED 20-FT. AND LINE UP WITH EXISTING JOINTS IN THE CURB & GUTTER.
 - FOR TYPE 1 & 2 REPAIRS ADJACENT TO CURB THAT IS NOT TO BE MODIFIED, REFER TO PAVEMENT DETAIL A ON DE-1.
 - REFER TO DE-02 FOR ADDITIONAL REINFORCING AROUND MANHOLE STRUCTURES. COST OF ADDITIONAL REINFORCEMENT SHALL BE INCIDENTAL TO PERTINENT CONCRETE REPAIRS.
 - CONCRETE REPAIRS, SIDEWALK REPLACEMENT AND CURB REPLACEMENT SHALL END 8 FEET FROM THE CENTERLINE OF ALL RAILROAD TRACKS.
 - NOTIFY "MISS UTILITY" 3 WORKING DAYS PRIOR TO EXCAVATION OR SIMILAR WORK IS TO BE PERFORMED.
 - MAINTAIN 10 FOOT CLEARANCE FROM OVERHEAD ELECTRIC LINES TO COMPLY WITH THE MARYLAND HIGH VOLTAGE LINE ACT.
 - DRIVEWAY APRON REPLACEMENT SHALL TIE IN TO EXISTING JOINT.

- LEGEND
- PAVEMENT REMOVAL
 - FULL SLAB REPLACEMENT
 - CONCRETE REPAIR
 - EXISTING TREE
 - PROPOSED TREE
 - CONCRETE SIDEWALK REPLACEMENT
 - CONCRETE DRIVEWAY APRON REPLACEMENT
 - EXISTING PAVEMENT JOINTS
 - 100' CRITICAL AREA BUFFER
 - DETECTABLE WARNING SURFACE (BC 655.40)

MATCH LINE STA. 110+50 - SEE SHEET PS-03

ADDENDUM NO. 1, PAGE 48 OF 73

327R

DRAWN BY _____
CHECKED BY _____



F.A.P. NO. AC-STBG-3241(5)E
 SHA NO. BC410024
 BALTIMORE CITY NO. TR17302

MATCH LINE STA. 118+50 - SEE SHEET PS-06

PS-05

CITY OF BALTIMORE
 DEPARTMENT OF TRANSPORTATION
 CONTRACT No. TR17302
 S. CLINTON STREET REHABILITATION
 FROM BOSTON STREET TO KEITH AVENUE
 ROADWAY PLAN

DATE: JULY 2019
 SHEET OF

SCALE: 1"=40'

CONCRETE REPAIR METHOD:
 (A) STD. NO. MD 577.02 METHOD A, TYPE I
 (B) STD. NO. MD 577.03 METHOD B, TYPE I
 (B2) STD. NO. MD 577.03 METHOD B, TYPE 2
 (C) STD. NO. MD 577.04 METHOD C, TYPE I
 (C2) STD. NO. MD 577.04 METHOD C, TYPE 2
 (D2) STD. NO. MD 577.05 METHOD D, TYPE 2
 (E) STD. NO. MD 577.06 METHOD E, TYPE I
 (E2) STD. NO. MD 577.06 METHOD E, TYPE 2

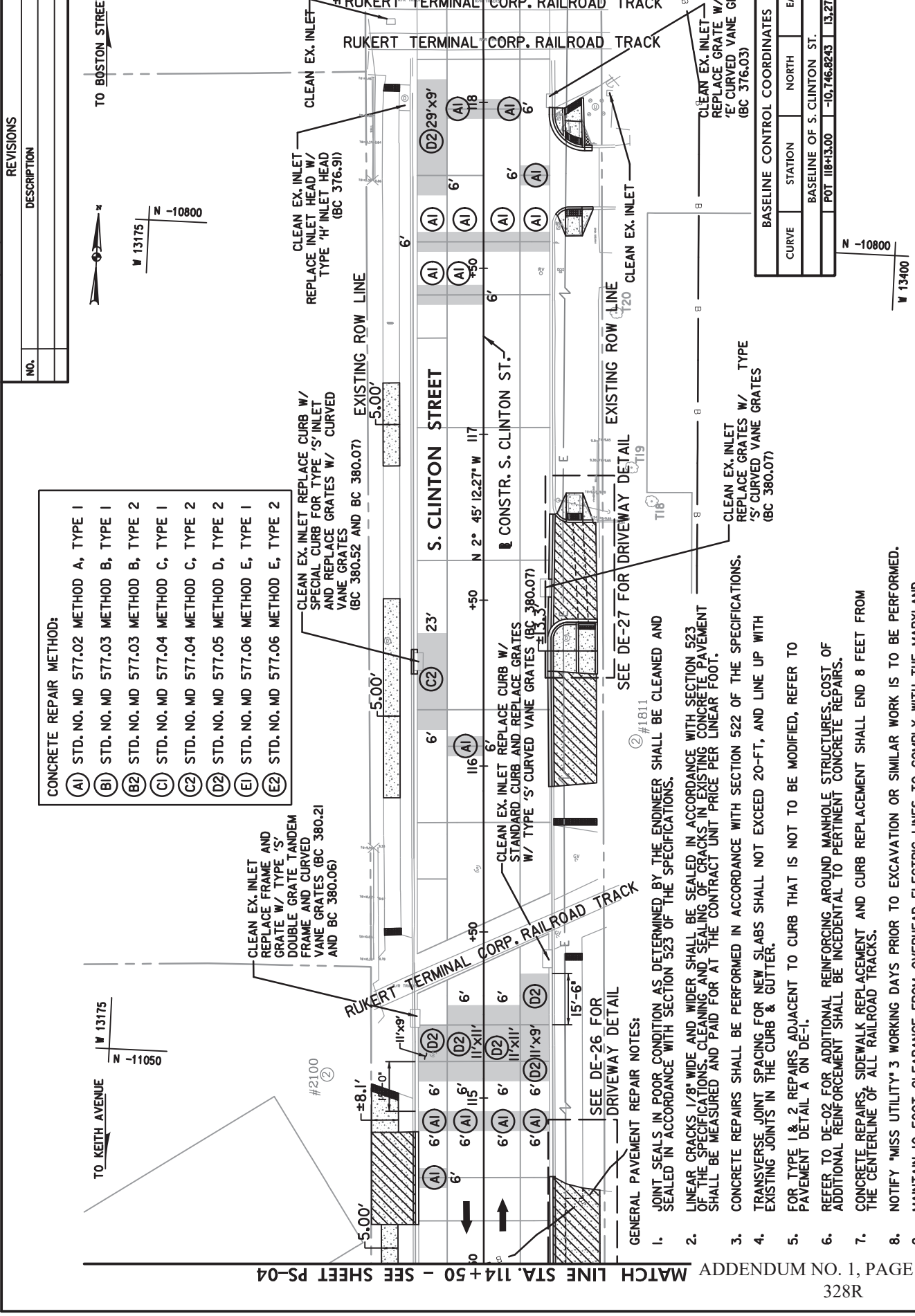
CONCRETE REPAIR NOTES:
 1. JOINT SEALS IN POOR CONDITION AS DETERMINED BY THE ENGINEER SHALL BE CLEANED AND SEALED IN ACCORDANCE WITH SECTION 523 OF THE SPECIFICATIONS.
 2. LINEAR CRACKS 1/8" WIDE AND WIDER SHALL BE SEALED IN ACCORDANCE WITH SECTION 523 OF THE SPECIFICATIONS. CLEANING AND SEALING OF CRACKS IN EXISTING CONCRETE PAVEMENT SHALL BE MEASURED AND PAID FOR AT THE CONTRACT UNIT PRICE PER LINEAR FOOT.
 3. CONCRETE REPAIRS SHALL BE PERFORMED IN ACCORDANCE WITH SECTION 522 OF THE SPECIFICATIONS.
 4. TRANSVERSE JOINT SPACING FOR NEW SLABS SHALL NOT EXCEED 20-FT. AND LINE UP WITH EXISTING JOINTS IN THE CURB & GUTTER.
 5. FOR TYPE 1 & 2 REPAIRS ADJACENT TO CURB THAT IS NOT TO BE MODIFIED, REFER TO PAVEMENT DETAIL A ON DE-1.
 6. REFER TO DE-02 FOR ADDITIONAL REINFORCING AROUND MANHOLE STRUCTURES. COST OF ADDITIONAL REINFORCEMENT SHALL BE INCIDENTAL TO PERTINENT CONCRETE REPAIRS.
 7. CONCRETE REPAIRS, SIDEWALK REPLACEMENT AND CURB REPLACEMENT SHALL END 8 FEET FROM THE CENTERLINE OF ALL RAILROAD TRACKS.
 8. NOTIFY "MISS UTILITY" 3 WORKING DAYS PRIOR TO EXCAVATION OR SIMILAR WORK IS TO BE PERFORMED.
 9. MAINTAIN 10 FOOT CLEARANCE FROM OVERHEAD ELECTRIC LINES TO COMPLY WITH THE MARYLAND HIGH VOLTAGE LINE ACT.
 10. DRIVEWAY APRON REPLACEMENT SHALL TIE IN TO EXISTING JOINT.

LEGEND

PAVEMENT REMOVAL
 PAVEMENT REMOVAL
 FULL SLAB REPLACEMENT
 CONCRETE REPAIR
 EXISTING TREE
 PROPOSED TREE

CONCRETE SIDEWALK REPLACEMENT
 CONCRETE DRIVEWAY APRON REPLACEMENT
 EXISTING PAVEMENT JOINTS
 100' CRITICAL AREA BUFFER

DETECTABLE WARNING SURFACE (BC 655.40)



DRAWN BY
 CHECKED BY



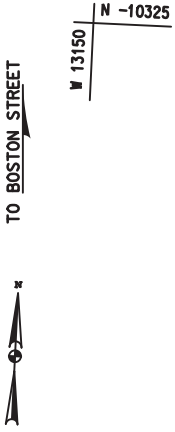
NO.	DESCRIPTION	DATE	BY

REVISIONS

MATCH LINE STA. 122+50 - SEE SHEET PS-07

F.A.P. NO. AC-STBG-3241(5)E
SHA NO. BC410024
BALTIMORE CITY NO. TR17302

PS-06



TO BOSTON STREET

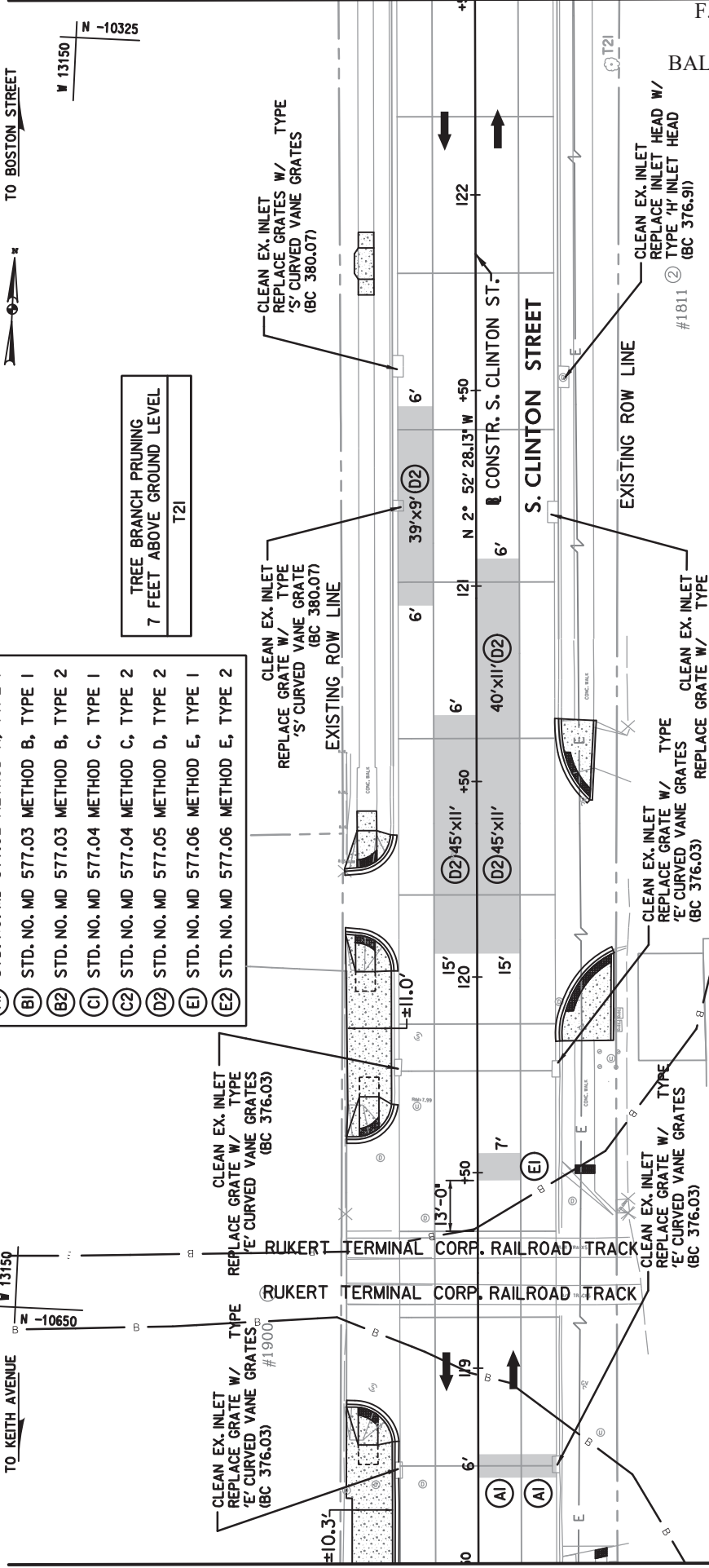
TO KEITH AVENUE

CONCRETE REPAIR METHOD:

(A1)	STD. NO. MD 577.02 METHOD A, TYPE 1
(B1)	STD. NO. MD 577.03 METHOD B, TYPE 1
(B2)	STD. NO. MD 577.03 METHOD B, TYPE 2
(C1)	STD. NO. MD 577.04 METHOD C, TYPE 1
(C2)	STD. NO. MD 577.04 METHOD C, TYPE 2
(D2)	STD. NO. MD 577.05 METHOD D, TYPE 2
(E1)	STD. NO. MD 577.06 METHOD E, TYPE 1
(E2)	STD. NO. MD 577.06 METHOD E, TYPE 2

TREE BRANCH PRUNING
7 FEET ABOVE GROUND LEVEL

T21



GENERAL PAVEMENT REPAIR NOTES:

- JOINT SEALS IN POOR CONDITION AS DETERMINED BY THE ENGINEER SHALL BE CLEANED AND SEALED IN ACCORDANCE WITH SECTION 523 OF THE SPECIFICATIONS.
- LINEAR CRACKS 1/8" WIDE AND WIDER SHALL BE SEALED IN ACCORDANCE WITH SECTION 523 OF THE SPECIFICATIONS. CLEANING AND SEALING OF CRACKS IN EXISTING CONCRETE PAVEMENT SHALL BE MEASURED AND PAID FOR AT THE CONTRACT UNIT PRICE PER LINEAR FOOT.
- CONCRETE REPAIRS SHALL BE PERFORMED IN ACCORDANCE WITH SECTION 522 OF THE SPECIFICATIONS.
- TRANSVERSE JOINT SPACING FOR NEW SLABS SHALL NOT EXCEED 20-FT., AND LINE UP WITH EXISTING JOINTS IN THE CURB & GUTTER.
- FOR TYPE I & 2 REPAIRS ADJACENT TO CURB THAT IS NOT TO BE MODIFIED, REFER TO PAVEMENT DETAIL A ON DE-1.
- REFER TO DE-02 FOR ADDITIONAL REINFORCING AROUND MANHOLE STRUCTURES. COST OF ADDITIONAL REINFORCEMENT SHALL BE INCIDENTAL TO PERTINENT CONCRETE REPAIRS.
- CONCRETE REPAIRS, SIDEWALK REPLACEMENT AND CURB REPLACEMENT SHALL END 8 FEET FROM THE CENTERLINE OF ALL RAILROAD TRACKS.
- NOTIFY "MISS UTILITY" 3 WORKING DAYS PRIOR TO EXCAVATION OR SIMILAR WORK IS TO BE PERFORMED.
- MAINTAIN 10 FOOT CLEARANCE FROM OVERHEAD ELECTRIC LINES TO COMPLY WITH THE MARYLAND HIGH VOLTAGE LINE ACT.
- DRIVEWAY APRON REPLACEMENT SHALL TIE IN TO EXISTING JOINT.

LEGEND

- PAVEMENT REMOVAL
- DETECTABLE WARNING SURFACE (BC 655.40)
- CONCRETE SIDEWALK REPLACEMENT
- CONCRETE DRIVEWAY APRON REPLACEMENT
- FULL SLAB REPLACEMENT
- CONCRETE REPAIR
- EXISTING PAVEMENT JOINTS
- EXISTING TREE
- PROPOSED TREE
- 100' CRITICAL AREA BUFFER

CITY OF BALTIMORE
DEPARTMENT OF TRANSPORTATION
CONTRACT No. TR17302
S. CLINTON STREET REHABILITATION
FROM BOSTON STREET TO KEITH AVENUE
ROADWAY PLAN

SCALE: 1"=40'

DATE: JULY 2019
SHEET



MATCH LINE STA. 118+50 - SEE SHEET PS-05

DRAWN BY
CHECKED BY

REVISIONS

NO.	DESCRIPTION	DATE	BY

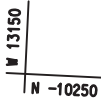
TO BOSTON STREET



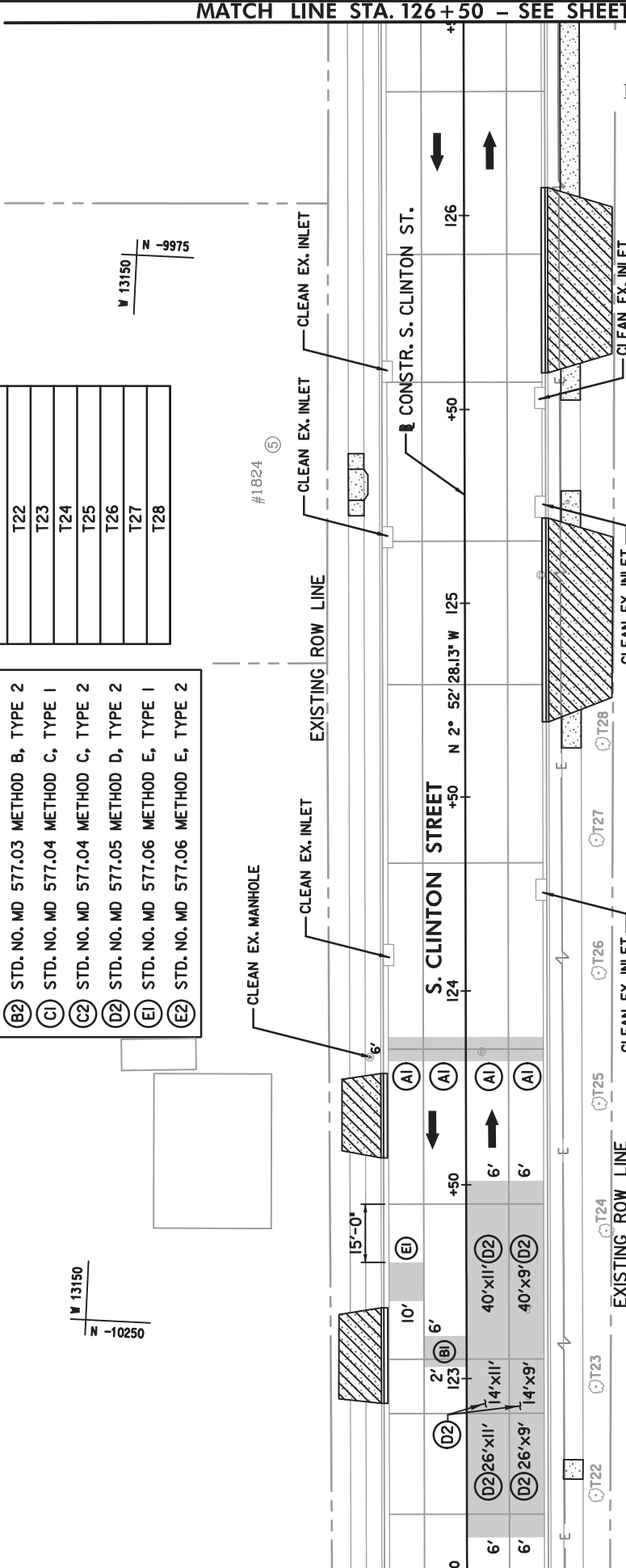
TREE BRANCH PRUNING 7 FEET ABOVE GROUND LEVEL
T22
T23
T24
T25
T26
T27
T28

CONCRETE REPAIR METHOD:

- (A) STD. NO. MD 577.02 METHOD A, TYPE 1
- (B) STD. NO. MD 577.03 METHOD B, TYPE 1
- (B2) STD. NO. MD 577.03 METHOD B, TYPE 2
- (C) STD. NO. MD 577.04 METHOD C, TYPE 1
- (C2) STD. NO. MD 577.04 METHOD C, TYPE 2
- (D) STD. NO. MD 577.05 METHOD D, TYPE 2
- (E) STD. NO. MD 577.06 METHOD E, TYPE 1
- (E2) STD. NO. MD 577.06 METHOD E, TYPE 2



TO KEITH AVENUE



GENERAL PAVEMENT REPAIR NOTES:

- JOINT SEALS IN POOR CONDITION AS DETERMINED BY THE ENGINEER SHALL BE CLEANED AND SEALED IN ACCORDANCE WITH SECTION 523 OF THE SPECIFICATIONS.
- LINEAR CRACKS 1/8" WIDE AND WIDER SHALL BE SEALED IN ACCORDANCE WITH SECTION 523 OF THE SPECIFICATIONS. CLEANING AND SEALING OF CRACKS IN EXISTING CONCRETE PAVEMENT SHALL BE MEASURED AND PAID FOR AT THE CONTRACT UNIT PRICE PER LINEAR FOOT.
- CONCRETE REPAIRS SHALL BE PERFORMED IN ACCORDANCE WITH SECTION 522 OF THE SPECIFICATIONS.
- TRANSVERSE JOINT SPACING FOR NEW SLABS SHALL NOT EXCEED 20-FT, AND LINE UP WITH EXISTING JOINTS IN THE CURB & GUTTER.
- FOR TYPE 1 & 2 REPAIRS ADJACENT TO CURB THAT IS NOT TO BE MODIFIED, REFER TO PAVEMENT DETAIL A ON DE-1.
- REFER TO DE-02 FOR ADDITIONAL REINFORCING AROUND MANHOLE STRUCTURES. COST OF ADDITIONAL REINFORCEMENT SHALL BE INCIDENTAL TO PERTINENT CONCRETE REPAIRS.
- CONCRETE REPAIRS, SIDEWALK REPLACEMENT AND CURB REPLACEMENT SHALL END 8 FEET FROM THE CENTERLINE OF ALL RAILROAD TRACKS.
- NOTIFY 'MISS UTILITY' 3 WORKING DAYS PRIOR TO EXCAVATION OR SIMILAR WORK IS TO BE PERFORMED.
- MAINTAIN 10 FOOT CLEARANCE FROM OVERHEAD ELECTRIC LINES TO COMPLY WITH THE MARYLAND HIGH VOLTAGE LINE ACT.
- DRIVEWAY APRON REPLACEMENT SHALL TIE IN TO EXISTING JOINT.

LEGEND

- CONCRETE SIDEWALK REPLACEMENT
- CONCRETE DRIVEWAY APRON REPLACEMENT
- EXISTING PAVEMENT JOINTS
- 100' CRITICAL AREA BUFFER

PAVEMENT REMOVAL

- DETECTABLE WARNING SURFACE (BC 655.40)
- FULL SLAB REPLACEMENT
- CONCRETE REPAIR
- EXISTING TREE
- PROPOSED TREE

CHECKED BY _____

DATE: JULY 2019

SHEET _____ OF _____

SCALE: 1"=40'

ROADWAY PLAN

S. CLINTON STREET REHABILITATION
FROM BOSTON STREET TO KEITH AVENUE

CONTRACT NO. TR17302

DEPARTMENT OF TRANSPORTATION

CITY OF BALTIMORE

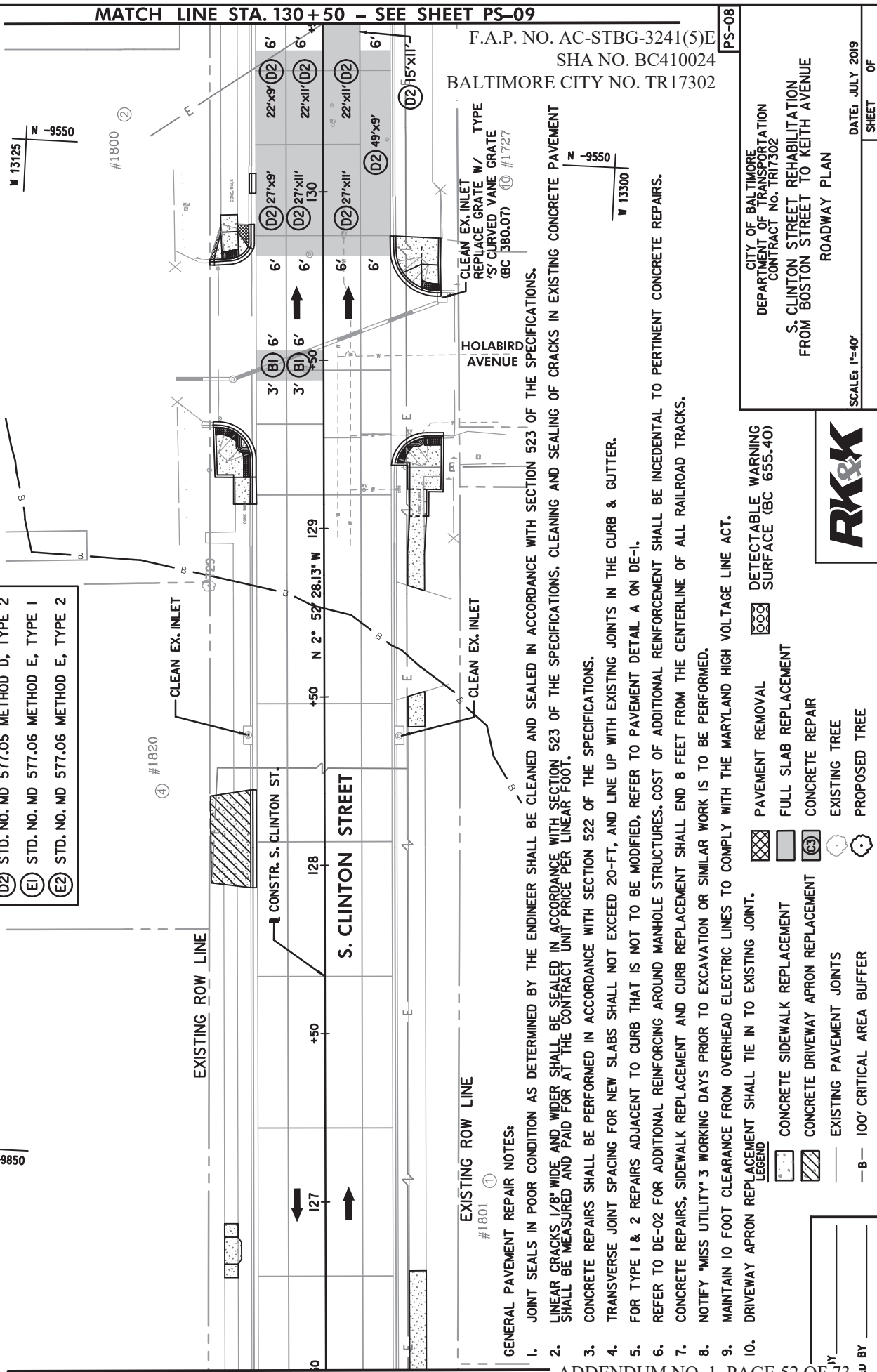
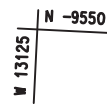
RK&K

NO.	DESCRIPTION	DATE	BY

TO KEITH AVENUE

TO BOSTON STREET

- CONCRETE REPAIR METHOD:**
- (A) STD. NO. MD 577.02 METHOD A, TYPE 1
 - (B) STD. NO. MD 577.03 METHOD B, TYPE 1
 - (B2) STD. NO. MD 577.03 METHOD B, TYPE 2
 - (C) STD. NO. MD 577.04 METHOD C, TYPE 1
 - (C2) STD. NO. MD 577.04 METHOD C, TYPE 2
 - (D) STD. NO. MD 577.05 METHOD D, TYPE 2
 - (E) STD. NO. MD 577.06 METHOD E, TYPE 1
 - (E2) STD. NO. MD 577.06 METHOD E, TYPE 2



F.A.P. NO. AC-STBG-3241(5)E
 SHA NO. BC410024
 BALTIMORE CITY NO. TR17302

- GENERAL PAVEMENT REPAIR NOTES:**
- JOINT SEALS IN POOR CONDITION AS DETERMINED BY THE ENGINEER SHALL BE CLEANED AND SEALED IN ACCORDANCE WITH SECTION 523 OF THE SPECIFICATIONS.
 - LINEAR CRACKS 1/8" WIDE AND WIDER SHALL BE SEALED IN ACCORDANCE WITH SECTION 523 OF THE SPECIFICATIONS. CLEANING AND SEALING OF CRACKS IN EXISTING CONCRETE PAVEMENT SHALL BE MEASURED AND PAID FOR AT THE CONTRACT UNIT PRICE PER LINEAR FOOT.
 - CONCRETE REPAIRS SHALL BE PERFORMED IN ACCORDANCE WITH SECTION 522 OF THE SPECIFICATIONS.
 - TRANSVERSE JOINT SPACING FOR NEW SLABS SHALL NOT EXCEED 20-FT, AND LINE UP WITH EXISTING JOINTS IN THE CURB & GUTTER.
 - FOR TYPE 1 & 2 REPAIRS ADJACENT TO CURB THAT IS NOT TO BE MODIFIED, REFER TO PAVEMENT DETAIL A ON DE-1.
 - REFER TO DE-02 FOR ADDITIONAL REINFORCING AROUND MANHOLE STRUCTURES. COST OF ADDITIONAL REINFORCEMENT SHALL BE INCIDENTAL TO PERTINENT CONCRETE REPAIRS.
 - CONCRETE REPAIRS, SIDEWALK REPLACEMENT AND CURB REPLACEMENT SHALL END 8 FEET FROM THE CENTERLINE OF ALL RAILROAD TRACKS.
 - NOTIFY "MISS UTILITY" 3 WORKING DAYS PRIOR TO EXCAVATION OR SIMILAR WORK IS TO BE PERFORMED.
 - MAINTAIN 10 FOOT CLEARANCE FROM OVERHEAD ELECTRIC LINES TO COMPLY WITH THE MARYLAND HIGH VOLTAGE LINE ACT.
 - DRIVEWAY APRON REPLACEMENT SHALL TIE IN TO EXISTING JOINT.

- LEGEND**
- PAVEMENT REMOVAL
 - CONCRETE SIDEWALK REPLACEMENT
 - CONCRETE DRIVEWAY APRON REPLACEMENT
 - EXISTING PAVEMENT JOINTS
 - 100' CRITICAL AREA BUFFER
 - DETECTABLE WARNING SURFACE (BC 655.40)
 - FULL SLAB REPLACEMENT
 - CONCRETE REPAIR
 - EXISTING TREE
 - PROPOSED TREE

CITY OF BALTIMORE
 DEPARTMENT OF TRANSPORTATION
 CONTRACT NO. TR17302
 S. CLINTON STREET REHABILITATION
 FROM BOSTON STREET TO KEITH AVENUE
 ROADWAY PLAN

DATE: JULY 2019
 SHEET OF

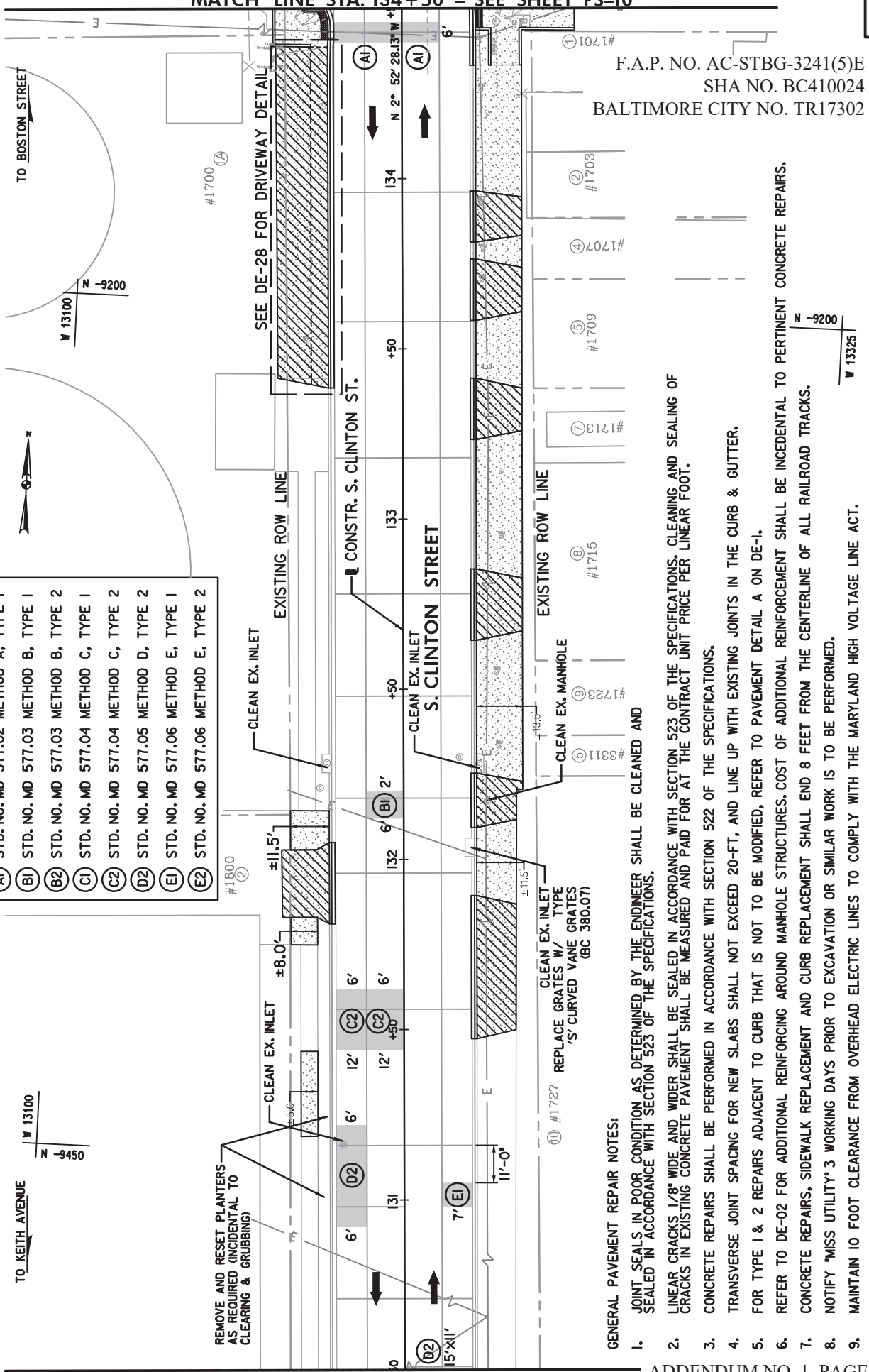


CHECKED BY _____
 BY _____

NO.	DESCRIPTION	DATE	BY

MATCH LINE STA. 134+50 - SEE SHEET PS-10

- CONCRETE REPAIR METHOD:**
- (A) STD. NO. MD 577.02 METHOD A, TYPE I
 - (B) STD. NO. MD 577.03 METHOD B, TYPE I
 - (B2) STD. NO. MD 577.03 METHOD B, TYPE 2
 - (C) STD. NO. MD 577.04 METHOD C, TYPE I
 - (C2) STD. NO. MD 577.04 METHOD C, TYPE 2
 - (D) STD. NO. MD 577.05 METHOD D, TYPE 2
 - (E) STD. NO. MD 577.06 METHOD E, TYPE I
 - (E2) STD. NO. MD 577.06 METHOD E, TYPE 2



F.A.P. NO. AC-STBG-3241(5)E
 SHA NO. BC410024
 BALTIMORE CITY NO. TR17302

- GENERAL PAVEMENT REPAIR NOTES:**
- JOINT SEALS IN POOR CONDITION AS DETERMINED BY THE ENGINEER SHALL BE CLEANED AND SEALED IN ACCORDANCE WITH SECTION 523 OF THE SPECIFICATIONS.
 - LINEAR CRACKS 1/8" WIDE AND WIDER SHALL BE SEALED IN ACCORDANCE WITH SECTION 523 OF THE SPECIFICATIONS. CLEANING AND SEALING OF CRACKS IN EXISTING CONCRETE PAVEMENT SHALL BE MEASURED AND PAID FOR AT THE CONTRACT UNIT PRICE PER LINEAR FOOT.
 - CONCRETE REPAIRS SHALL BE PERFORMED IN ACCORDANCE WITH SECTION 522 OF THE SPECIFICATIONS.
 - TRANSVERSE JOINT SPACING FOR NEW SLABS SHALL NOT EXCEED 20-FT, AND LINE UP WITH EXISTING JOINTS IN THE CURB & GUTTER.
 - FOR TYPE 1 & 2 REPAIRS ADJACENT TO CURB THAT IS NOT TO BE MODIFIED, REFER TO PAVEMENT DETAIL A ON DE-1.
 - REFER TO DE-02 FOR ADDITIONAL REINFORCING AROUND MANHOLE STRUCTURES. COST OF ADDITIONAL REINFORCEMENT SHALL BE INCIDENTAL TO PERTINENT CONCRETE REPAIRS.
 - CONCRETE REPAIRS, SIDEWALK REPLACEMENT AND CURB REPLACEMENT SHALL END 8 FEET FROM THE CENTERLINE OF ALL RAILROAD TRACKS.
 - NOTIFY "MISS UTILITY" 3 WORKING DAYS PRIOR TO EXCAVATION OR SIMILAR WORK IS TO BE PERFORMED.
 - MAINTAIN 10 FOOT CLEARANCE FROM OVERHEAD ELECTRIC LINES TO COMPLY WITH THE MARYLAND HIGH VOLTAGE LINE ACT.
 - DRIVEWAY APRON REPLACEMENT SHALL TIE IN TO EXISTING JOINT.

- LEGEND**
- PAVEMENT REMOVAL
 - DETECTABLE WARNING SURFACE (BC 655.40)
 - CONCRETE SIDEWALK REPLACEMENT
 - CONCRETE DRIVEWAY APRON REPLACEMENT
 - EXISTING PAVEMENT JOINTS
 - 100' CRITICAL AREA BUFFER
 - FULL SLAB REPLACEMENT
 - CONCRETE REPAIR
 - EXISTING TREE
 - PROPOSED TREE

MATCH LINE STA. 130+50 - SEE SHEET PS-08

ADDENDUM NO. 1, PAGE 53 OF 73
 332R

DRAWN BY _____
 CHECKED BY _____

CITY OF BALTIMORE
 DEPARTMENT OF TRANSPORTATION
 CONTRACT NO. TR17302
 S. CLINTON STREET REHABILITATION
 FROM BOSTON STREET TO KEITH AVENUE
 ROADWAY PLAN

DATE: JULY 2019
 SHEET OF

SCALE: 1"=40'



MATCH LINE STA. 138+00 - SEE SHEET PS-11

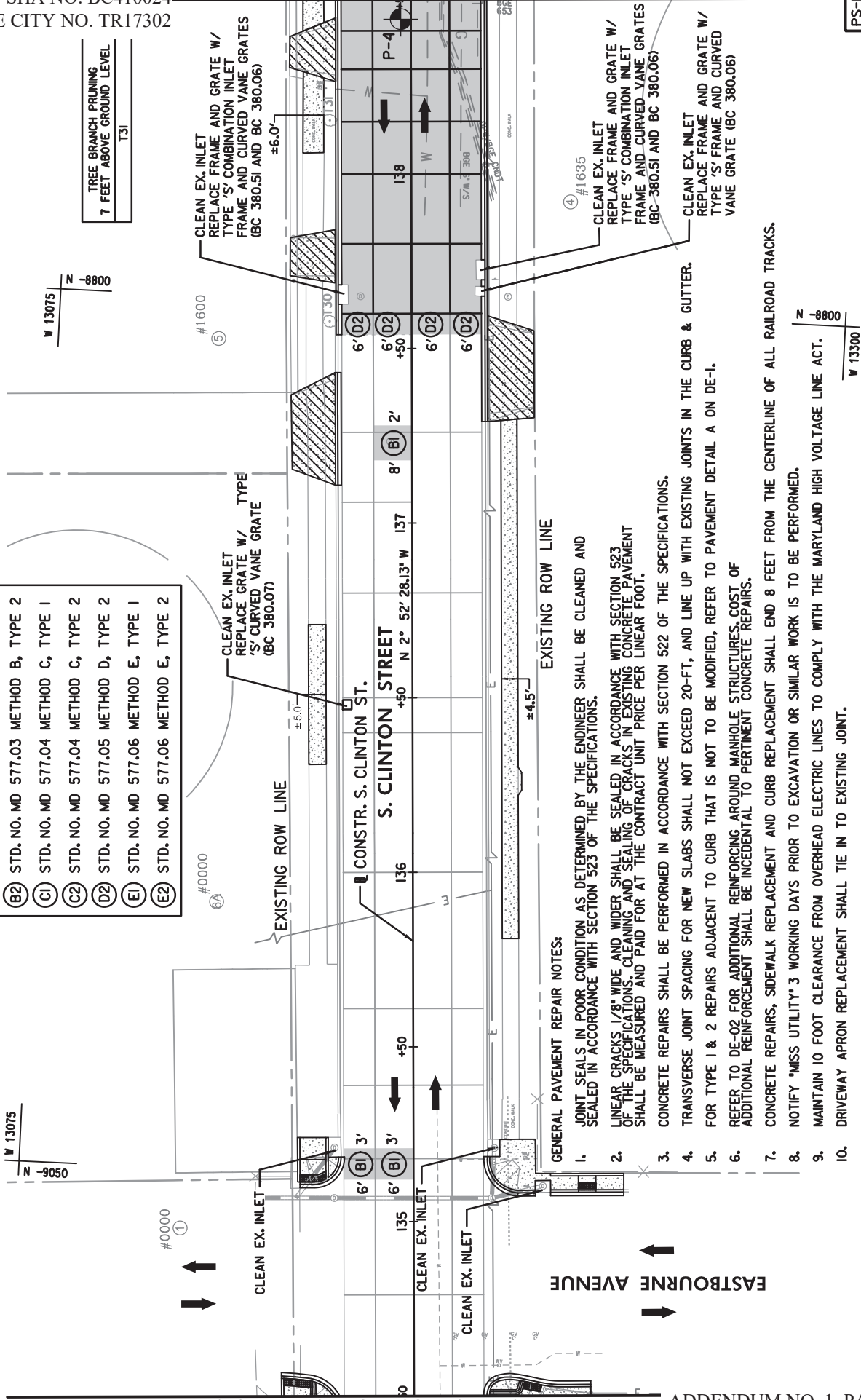
NO.	DESCRIPTION	DATE

TO BOSTON STREET



TREE BRANCH PRUNING
 7 FEET ABOVE GROUND LEVEL
 T31

- CONCRETE REPAIR METHOD:**
- (A) STD. NO. MD 577.02 METHOD A, TYPE 1
 - (B) STD. NO. MD 577.03 METHOD B, TYPE 1
 - (B2) STD. NO. MD 577.03 METHOD B, TYPE 2
 - (C) STD. NO. MD 577.04 METHOD C, TYPE 1
 - (C2) STD. NO. MD 577.04 METHOD C, TYPE 2
 - (D) STD. NO. MD 577.05 METHOD D, TYPE 2
 - (E) STD. NO. MD 577.06 METHOD E, TYPE 1
 - (E2) STD. NO. MD 577.06 METHOD E, TYPE 2



- GENERAL PAVEMENT REPAIR NOTES:**
- JOINT SEALS IN POOR CONDITION AS DETERMINED BY THE ENGINEER SHALL BE CLEANED AND SEALED IN ACCORDANCE WITH SECTION 523 OF THE SPECIFICATIONS.
 - LINEAR CRACKS 1/8" WIDE AND WIDER SHALL BE SEALED IN ACCORDANCE WITH SECTION 523 OF THE SPECIFICATIONS. CLEANING AND SEALING OF CRACKS IN EXISTING CONCRETE PAVEMENT SHALL BE MEASURED AND PAID FOR AT THE CONTRACT UNIT PRICE PER LINEAR FOOT.
 - CONCRETE REPAIRS SHALL BE PERFORMED IN ACCORDANCE WITH SECTION 522 OF THE SPECIFICATIONS.
 - TRANSVERSE JOINT SPACING FOR NEW SLABS SHALL NOT EXCEED 20'-FT., AND LINE UP WITH EXISTING JOINTS IN THE CURB & GUTTER.
 - FOR TYPE 1 & 2 REPAIRS ADJACENT TO CURB THAT IS NOT TO BE MODIFIED, REFER TO PAVEMENT DETAIL A ON DE-1.
 - REFER TO DE-02 FOR ADDITIONAL REINFORCING AROUND MANHOLE STRUCTURES. COST OF ADDITIONAL REINFORCEMENT SHALL BE INCIDENTAL TO PERTINENT CONCRETE REPAIRS.
 - CONCRETE REPAIRS, SIDEWALK REPLACEMENT AND CURB REPLACEMENT SHALL END 8 FEET FROM THE CENTERLINE OF ALL RAILROAD TRACKS.
 - NOTIFY "MISS UTILITY" 3 WORKING DAYS PRIOR TO EXCAVATION OR SIMILAR WORK IS TO BE PERFORMED.
 - MAINTAIN 10 FOOT CLEARANCE FROM OVERHEAD ELECTRIC LINES TO COMPLY WITH THE MARYLAND HIGH VOLTAGE LINE ACT.
 - DRIVEWAY APRON REPLACEMENT SHALL TIE IN TO EXISTING JOINT.

- LEGEND**
- PAVEMENT REMOVAL
 - CONCRETE SIDEWALK REPLACEMENT
 - CONCRETE DRIVEWAY APRON REPLACEMENT
 - EXISTING PAVEMENT JOINTS
 - 100' CRITICAL AREA BUFFER
 - PAVEMENT REMOVAL
 - FULL SLAB REPLACEMENT
 - CONCRETE REPAIR
 - EXISTING TREE
 - PROPOSED TREE
 - DETECTABLE WARNING SURFACE (BC 655.40)

MATCH LINE STA. 134+50 - SEE SHEET PS-09

ADDENDUM NO. 1, PAGE 54 OF 73
 333R

PS-10

CITY OF BALTIMORE
 DEPARTMENT OF TRANSPORTATION
 CONTRACT NO. TR17302
 S. CLINTON STREET REHABILITATION
 FROM BOSTON STREET TO KEITH AVENUE
 ROADWAY PLAN



SCALE: 1"=40'

DATE: JULY 2019
 SHEET

CHECKED BY

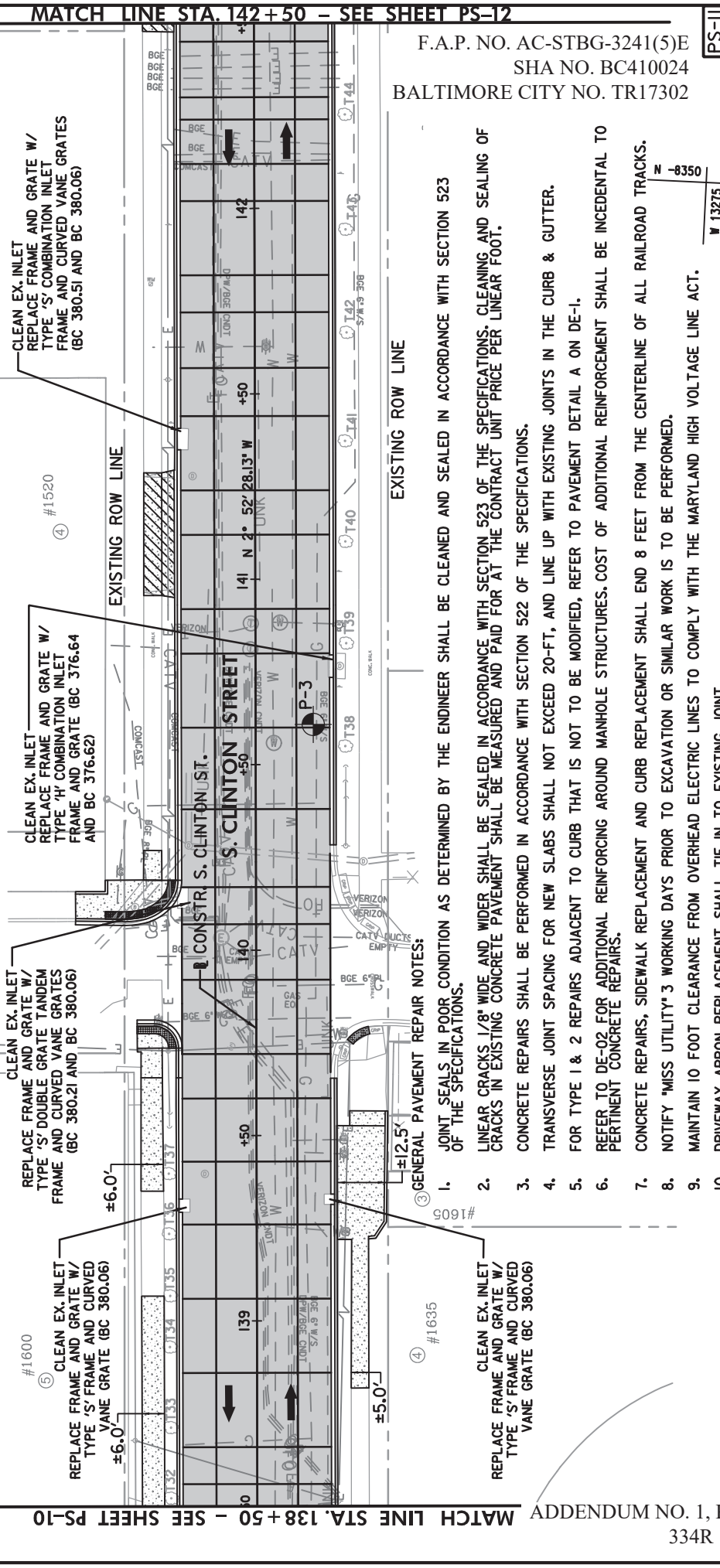
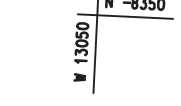
NO.	DESCRIPTION	DATE	BY

CONCRETE REPAIR METHOD:	
(A)	STD. NO. MD 577.02 METHOD A, TYPE 1
(B)	STD. NO. MD 577.03 METHOD B, TYPE 1
(B2)	STD. NO. MD 577.03 METHOD B, TYPE 2
(C)	STD. NO. MD 577.04 METHOD C, TYPE 1
(C2)	STD. NO. MD 577.04 METHOD C, TYPE 2
(D2)	STD. NO. MD 577.05 METHOD D, TYPE 2
(E)	STD. NO. MD 577.06 METHOD E, TYPE 1
(E2)	STD. NO. MD 577.06 METHOD E, TYPE 2

TO BOSTON STREET

TO KEITH AVENUE

TREE BRANCH PRUNING 7 FEET ABOVE GROUND LEVEL
T32
T33
T34
T35
T36
T37



F.A.P. NO. AC-STBG-3241(5)E
SHA NO. BC410024
BALTIMORE CITY NO. TR17302

EXISTING ROW LINE

GENERAL PAVEMENT REPAIR NOTES:

1. JOINT SEALS IN POOR CONDITION AS DETERMINED BY THE ENGINEER SHALL BE CLEANED AND SEALED IN ACCORDANCE WITH SECTION 523 OF THE SPECIFICATIONS.
2. LINEAR CRACKS 1/8" WIDE AND WIDER SHALL BE SEALED IN ACCORDANCE WITH SECTION 523 OF THE SPECIFICATIONS. CLEANING AND SEALING OF CRACKS IN EXISTING CONCRETE PAVEMENT SHALL BE MEASURED AND PAID FOR AT THE CONTRACT UNIT PRICE PER LINEAR FOOT.
3. CONCRETE REPAIRS SHALL BE PERFORMED IN ACCORDANCE WITH SECTION 522 OF THE SPECIFICATIONS.
4. TRANSVERSE JOINT SPACING FOR NEW SLABS SHALL NOT EXCEED 20-FT, AND LINE UP WITH EXISTING JOINTS IN THE CURB & GUTTER.
5. FOR TYPE 1 & 2 REPAIRS ADJACENT TO CURB THAT IS NOT TO BE MODIFIED, REFER TO PAVEMENT DETAIL A ON DE-1.
6. REFER TO DE-02 FOR ADDITIONAL REINFORCING AROUND MANHOLE STRUCTURES. COST OF ADDITIONAL REINFORCEMENT SHALL BE INCIDENTAL TO CONCRETE REPAIRS.
7. CONCRETE REPAIRS, SIDEWALK REPLACEMENT AND CURB REPLACEMENT SHALL END 8 FEET FROM THE CENTERLINE OF ALL RAILROAD TRACKS.
8. NOTIFY 'MISS UTILITY' 3 WORKING DAYS PRIOR TO EXCAVATION OR SIMILAR WORK IS TO BE PERFORMED.
9. MAINTAIN 10 FOOT CLEARANCE FROM OVERHEAD ELECTRIC LINES TO COMPLY WITH THE MARYLAND HIGH VOLTAGE LINE ACT.
10. DRIVEWAY APRON REPLACEMENT SHALL TIE IN TO EXISTING JOINT.

LEGEND

	CONCRETE SIDEWALK REPLACEMENT
	CONCRETE DRIVEWAY APRON REPLACEMENT
	EXISTING PAVEMENT JOINTS
	100' CRITICAL AREA BUFFER

	PAVEMENT REMOVAL
	FULL SLAB REPLACEMENT
	CONCRETE REPAIR
	EXISTING TREE
	PROPOSED TREE

DETECTABLE WARNING SURFACE (BC 655.40)



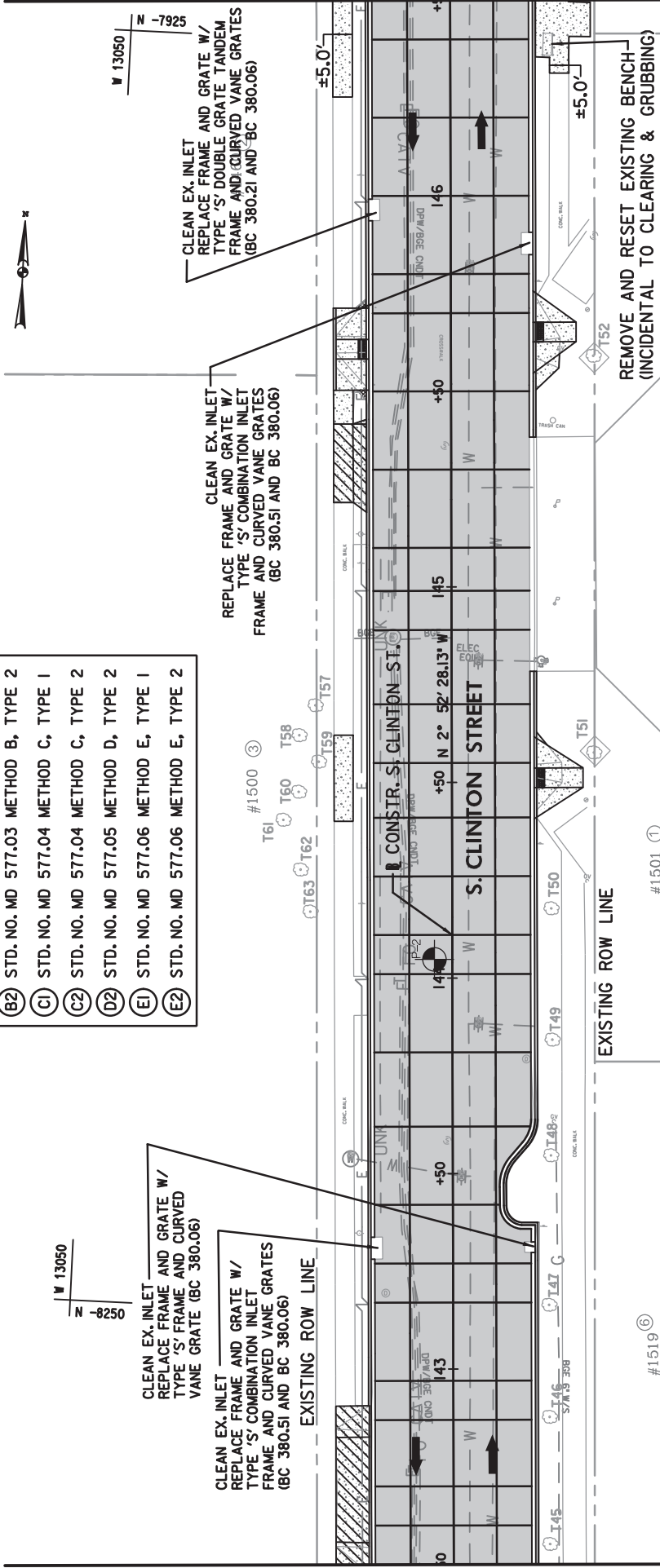
CITY OF BALTIMORE
DEPARTMENT OF TRANSPORTATION
CONTRACT NO. TR17302
S. CLINTON STREET REHABILITATION
FROM BOSTON STREET TO KEITH AVENUE
ROADWAY PLAN
SCALE: 1"=40'
DATE: JULY 2019
SHEET OF

NO.	DESCRIPTION	DATE	BY

MATCH LINE STA. 146 + 50 - SEE SHEET PS-13

TO BOSTON STREET

TO KEITH AVENUE



F.A.P. NO. AC-STBG-3241(5)E
 SHA NO. BC410024
 BALTIMORE CITY NO. TR17302

PS-12

CITY OF BALTIMORE
 DEPARTMENT OF TRANSPORTATION
 CONTRACT No. TR17302
 S. CLINTON STREET REHABILITATION
 FROM BOSTON STREET TO KEITH AVENUE
 ROADWAY PLAN

SCALE: 1"=40'

DATE: JULY 2019
 SHEET OF

CONCRETE REPAIR METHOD:

(A)	STD. NO. MD 577.02 METHOD A, TYPE 1
(B)	STD. NO. MD 577.03 METHOD B, TYPE 1
(B2)	STD. NO. MD 577.03 METHOD B, TYPE 2
(C)	STD. NO. MD 577.04 METHOD C, TYPE 1
(C2)	STD. NO. MD 577.04 METHOD C, TYPE 2
(D2)	STD. NO. MD 577.05 METHOD D, TYPE 2
(E)	STD. NO. MD 577.06 METHOD E, TYPE 1
(E2)	STD. NO. MD 577.06 METHOD E, TYPE 2

CLEAN EX. INLET
 REPLACE FRAME AND GRATE W/
 TYPE 'S' FRAME AND CURVED
 VANE GRATE (BC 380.06)

CLEAN EX. INLET
 REPLACE FRAME AND GRATE W/
 TYPE 'S' COMBINATION INLET
 FRAME AND CURVED VANE GRATES
 (BC 380.51 AND BC 380.06)

EXISTING ROW LINE

EXISTING ROW LINE

GENERAL PAVEMENT REPAIR NOTES:

- JOINT SEALS IN POOR CONDITION AS DETERMINED BY THE ENGINEER SHALL BE CLEANED AND SEALED IN ACCORDANCE WITH SECTION 523 OF THE SPECIFICATIONS.
- LINEAR CRACKS 1/8" WIDE AND WIDER SHALL BE SEALED IN ACCORDANCE WITH SECTION 523 OF THE SPECIFICATIONS. CLEANING AND SEALING OF CRACKS IN EXISTING CONCRETE PAVEMENT SHALL BE MEASURED AND PAID FOR AT THE CONTRACT UNIT PRICE PER LINEAR FOOT.
- CONCRETE REPAIRS SHALL BE PERFORMED IN ACCORDANCE WITH SECTION 522 OF THE SPECIFICATIONS.
- TRANSVERSE JOINT SPACING FOR NEW SLABS SHALL NOT EXCEED 20-FT, AND LINE UP WITH EXISTING JOINTS IN THE CURB & GUTTER.
- FOR TYPE 1 & 2 REPAIRS ADJACENT TO CURB THAT IS NOT TO BE MODIFIED, REFER TO PAVEMENT DETAIL A ON DE-1.
- REFER TO DE-02 FOR ADDITIONAL REINFORCING AROUND MANHOLE STRUCTURES. COST OF ADDITIONAL REINFORCEMENT SHALL BE INCIDENTAL TO PERTINENT CONCRETE REPAIRS.
- CONCRETE REPAIRS, SIDEWALK REPLACEMENT AND CURB REPLACEMENT SHALL END 8 FEET FROM THE CENTERLINE OF ALL RAILROAD TRACKS.
- NOTIFY "MISS UTILITY" 3 WORKING DAYS PRIOR TO EXCAVATION OR SIMILAR WORK IS TO BE PERFORMED.
- MAINTAIN 10 FOOT CLEARANCE FROM OVERHEAD ELECTRIC LINES TO COMPLY WITH THE MARYLAND HIGH VOLTAGE LINE ACT.
- DRIVEWAY APRON REPLACEMENT SHALL TIE IN TO EXISTING JOINT.

LEGEND

	PAVEMENT REMOVAL		DETECTABLE WARNING SURFACE (BC 655.40)
	CONCRETE SIDEWALK REPLACEMENT		FULL SLAB REPLACEMENT
	CONCRETE DRIVEWAY APRON REPLACEMENT		CONCRETE REPAIR
	EXISTING PAVEMENT JOINTS		EXISTING TREE
	100' CRITICAL AREA BUFFER		PROPOSED TREE



MATCH LINE STA. 142 + 50 - SEE SHEET PS-11

ADDENDUM NO. 1, PAGE 56 OF 73
 335R

CHECKED BY _____
 JY _____

REVISIONS

NO. DESCRIPTION

TREE FELLING OPERATION I
T53

CONCRETE REPAIR METHOD:

(A)	STD. NO. MD 577.02	METHOD A, TYPE 1
(B)	STD. NO. MD 577.03	METHOD B, TYPE 1
(B2)	STD. NO. MD 577.03	METHOD B, TYPE 2
(C)	STD. NO. MD 577.04	METHOD C, TYPE 1
(C2)	STD. NO. MD 577.04	METHOD C, TYPE 2
(D2)	STD. NO. MD 577.05	METHOD D, TYPE 2
(E)	STD. NO. MD 577.06	METHOD E, TYPE 1
(E2)	STD. NO. MD 577.06	METHOD E, TYPE 2

N 13000
 N -7800

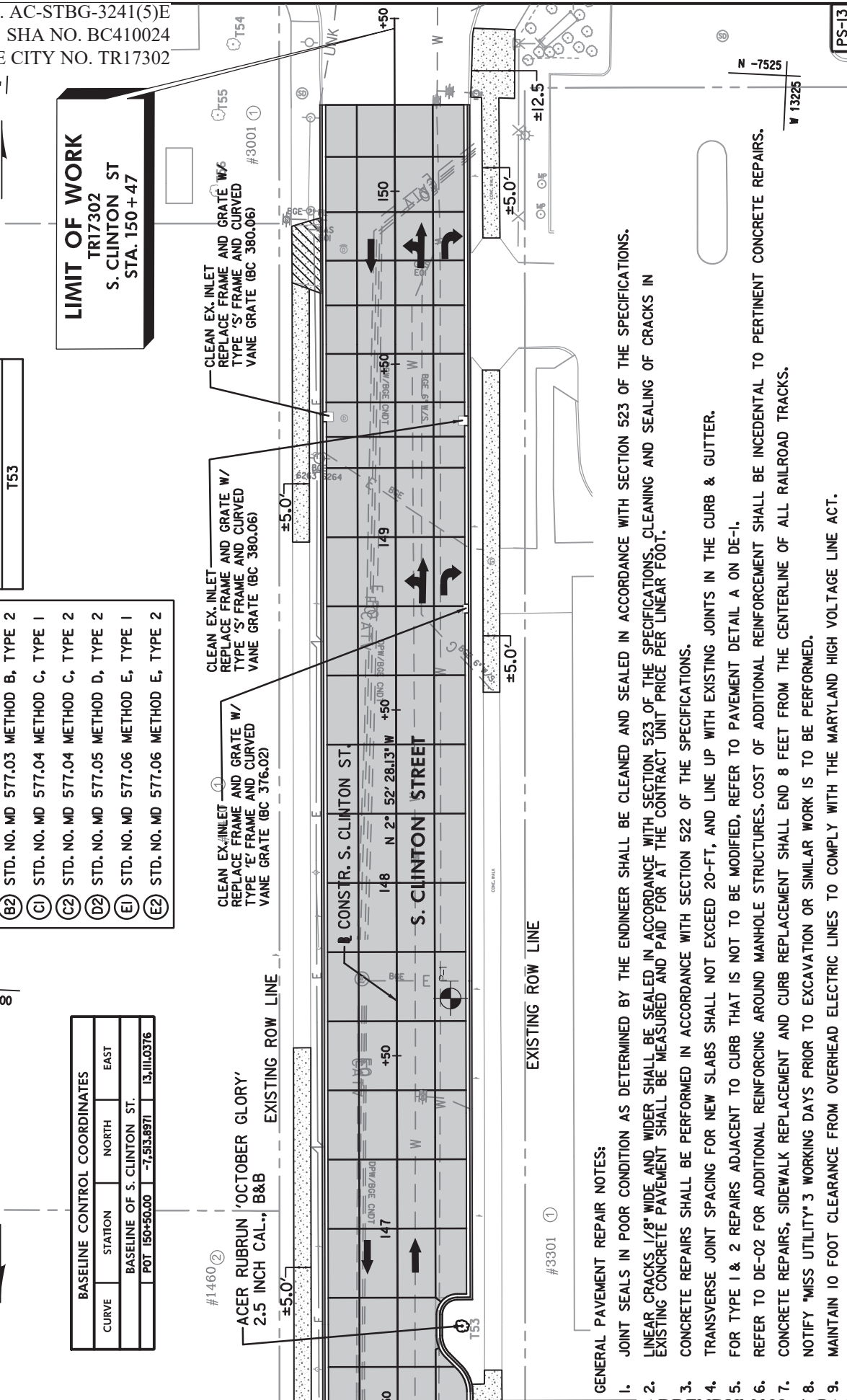
TO KEITH AVENUE

TO BOSTON STREET

BASELINE CONTROL COORDINATES		
CURVE	STATION	NORTH
		EAST
BASELINE OF S. CLINTON ST.		
POT	150+50.00	-7,513.8971
		13,111.0376

LIMIT OF WORK
 TR17302
 S. CLINTON ST
 STA. 150 +47

MATCH LINE STA. 146 + 50 - SEE SHEET PS-12



GENERAL PAVEMENT REPAIR NOTES:

- JOINT SEALS IN POOR CONDITION AS DETERMINED BY THE ENGINEER SHALL BE CLEANED AND SEALED IN ACCORDANCE WITH SECTION 523 OF THE SPECIFICATIONS.
- LINEAR CRACKS 1/8" WIDE AND WIDER SHALL BE SEALED IN ACCORDANCE WITH SECTION 523 OF THE SPECIFICATIONS, CLEANING AND SEALING OF CRACKS IN EXISTING CONCRETE PAVEMENT SHALL BE MEASURED AND PAID FOR AT THE CONTRACT UNIT PRICE PER LINEAR FOOT.
- CONCRETE REPAIRS SHALL BE PERFORMED IN ACCORDANCE WITH SECTION 522 OF THE SPECIFICATIONS.
- TRANSVERSE JOINT SPACING FOR NEW SLABS SHALL NOT EXCEED 20-FT, AND LINE UP WITH EXISTING JOINTS IN THE CURB & GUTTER.
- FOR TYPE 1 & 2 REPAIRS ADJACENT TO CURB THAT IS NOT TO BE MODIFIED, REFER TO PAVEMENT DETAIL A ON DE-1.
- REFER TO DE-02 FOR ADDITIONAL REINFORCING AROUND MANHOLE STRUCTURES. COST OF ADDITIONAL REINFORCEMENT SHALL BE INCIDENTAL TO PERTINENT CONCRETE REPAIRS.
- CONCRETE REPAIRS, SIDEWALK REPLACEMENT AND CURB REPLACEMENT SHALL END 8 FEET FROM THE CENTERLINE OF ALL RAILROAD TRACKS.
- NOTIFY "MISS UTILITY" 3 WORKING DAYS PRIOR TO EXCAVATION OR SIMILAR WORK IS TO BE PERFORMED.
- MAINTAIN 10 FOOT CLEARANCE FROM OVERHEAD ELECTRIC LINES TO COMPLY WITH THE MARYLAND HIGH VOLTAGE LINE ACT.
- DRIVEWAY APRON REPLACEMENT SHALL TIE IN TO EXISTING JOINT.

LEGEND

- PAVEMENT REMOVAL
- DETECTABLE WARNING SURFACE (BC 655.40)
- CONCRETE SIDEWALK REPLACEMENT
- CONCRETE DRIVEWAY APRON REPLACEMENT
- EXISTING PAVEMENT JOINTS
- 100' CRITICAL AREA BUFFER
- PAVEMENT REMOVAL
- FULL SLAB REPLACEMENT
- CONCRETE REPAIR
- EXISTING TREE
- PROPOSED TREE

CHECKED BY _____
 DATE: JULY 2020



CITY OF BALTIMORE
 DEPARTMENT OF TRANSPORTATION
 CONTRACT NO. TR17302
 S. CLINTON STREET REHABILITATION
 FROM BOSTON STREET TO KEITH AVENUE
 ROADWAY PLAN
 SCALE: 1"=40'
 DATE: JULY 2019
 SHEET OF

SN-2.01

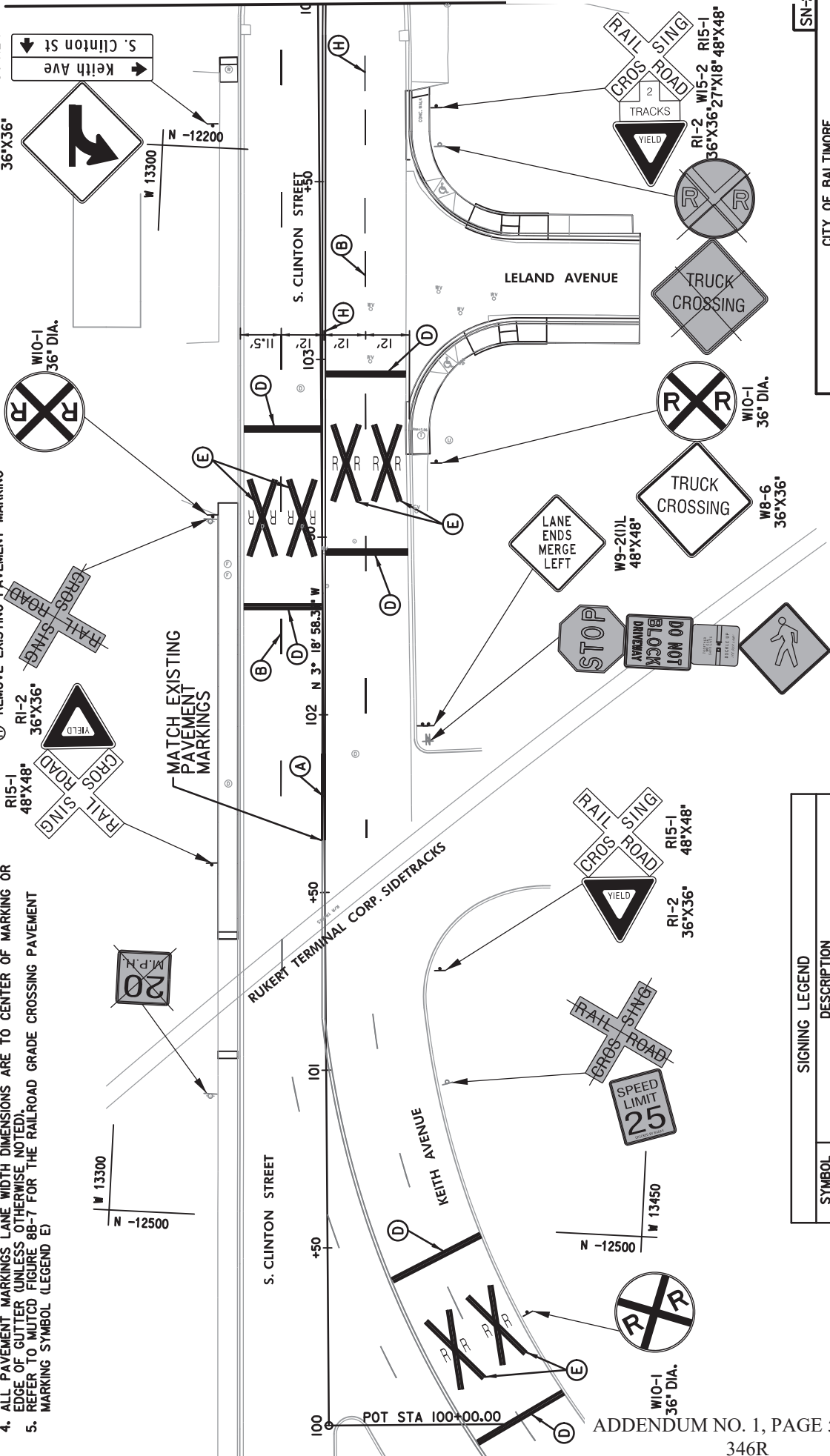
NO.	REVISIONS	DATE	BY

PAVEMENT MARKING LEGEND

- A 5 INCH SOLID DOUBLE YELLOW CONTRAST PAVEMENT MARKING TAPE
- B 5 INCH SOLID WHITE CONTRAST PAVEMENT MARKING TAPE (10' STRIPE, 30' GAP)
- C 5 INCH SOLID WHITE CONTRAST PAVEMENT MARKING TAPE
- D 24 INCH SOLID WHITE PREFORMED THERMOPLASTIC PAVEMENT MARKING
- E RAILROAD GRADE CROSSING PAVEMENT MARKING SYMBOL
- F WHITE PREFORMED THERMOPLASTIC PAVEMENT MARKING LEGENDS AND SYMBOLS
- G 5 INCH WHITE CONTRAST PAVEMENT MARKING TAPE (3' STRIPE, 10' GAP)
- H REMOVE EXISTING PAVEMENT MARKING

SIGNING AND PAVEMENT MARKING GENERAL NOTES

1. THE CONTRACTOR SHALL CENTER THE PROPOSED CROSSWALKS ON THE RAMPS.
2. THE CONTRACTOR SHALL INSTALL STOPLINES A MINIMUM OF 4 FEET BEHIND CROSSWALKS (EDGE-TO-EDGE, NOT CENTER-TO-CENTER), PERPENDICULAR TO FACE OF CURB, UNLESS OTHERWISE NOTED.
3. SIGN SUPPORTS INSTALLED IN CONCRETE OR BRICK SHALL BE INSTALLED ON SQUARE PERFORATED TUBULAR STEEL POSTS WITH SLEEVED FOUNDATIONS.
4. ALL PAVEMENT MARKINGS LANE WIDTH DIMENSIONS ARE TO CENTER OF MARKING OR EDGE OF GUTTER (UNLESS OTHERWISE NOTED).
5. REFER TO MUTCD FIGURE 8B-7 FOR THE RAILROAD GRADE CROSSING PAVEMENT MARKING SYMBOL (LEGEND E)



SYMBOL	DESCRIPTION
	EXISTING GROUND MOUNTED SIGN AND SUPPORT(S)
	PROPOSED GROUND MOUNTED SIGN AND SUPPORT(S)
	EXISTING SIGN TO REMAIN
	EXISTING SIGN TO BE REMOVED
	PROPOSED SIGN TO BE INSTALLED

CITY OF BALTIMORE
DEPARTMENT OF TRANSPORTATION
CONTRACT NO. TR17302
SOUTH CLINTON STREET REHABILITATION
FROM BOSTON STREET TO KEITH AVENUE
SIGNING AND MARKING PLAN



SCALE: N.T.S

DATE: DECEMBER 2018
SHEET 01 OF 13

DRAWN BY _____
CHECKED BY _____

NO.	REVISIONS	DATE	BY

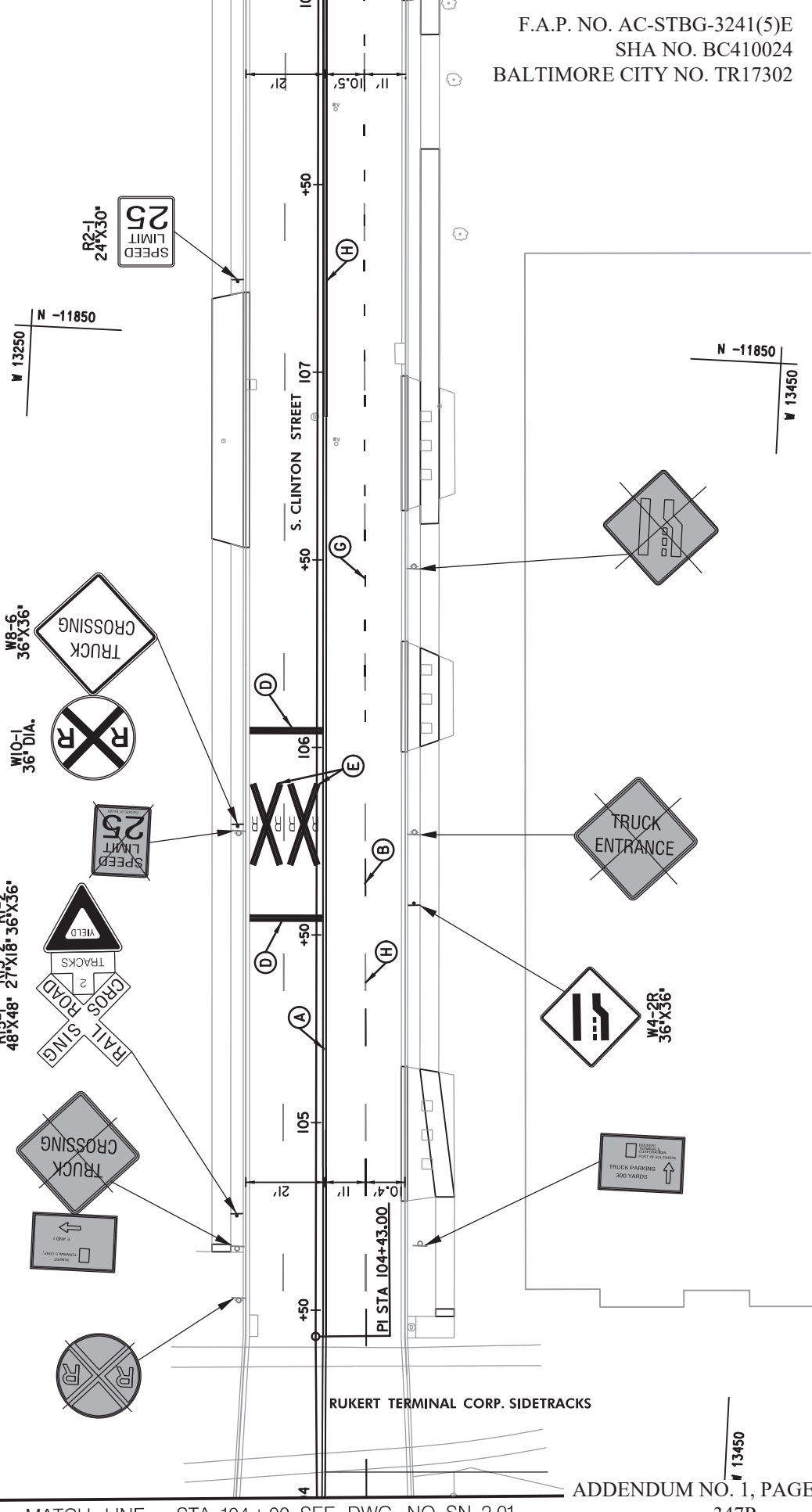
MATCH LINE - STA. 108+00, SEE DWG NO. SN-2.03

PAVEMENT MARKING LEGEND

- (A) 5 INCH SOLID DOUBLE YELLOW CONTRAST PAVEMENT MARKING TAPE
- (B) 5 INCH SOLID WHITE CONTRAST PAVEMENT MARKING TAPE (10' STRIPE, 30' GAP)
- (C) 5 INCH SOLID WHITE CONTRAST PAVEMENT MARKING TAPE
- (D) 24 INCH SOLID WHITE PREFORMED THERMOPLASTIC PAVEMENT MARKING
- (E) RAILROAD GRADE CROSSING PAVEMENT MARKING SYMBOL
- (F) WHITE PREFORMED THERMOPLASTIC PAVEMENT MARKING LEGENDS AND SYMBOLS
- (G) 5 INCH WHITE CONTRAST PAVEMENT MARKING TAPE (3' STRIPE, 10' GAP)
- (H) REMOVE EXISTING PAVEMENT MARKING

SIGNING AND PAVEMENT MARKING GENERAL NOTES

1. THE CONTRACTOR SHALL CENTER THE PROPOSED CROSSWALKS ON THE RAMPS.
2. THE CONTRACTOR SHALL INSTALL STOPLINES A MINIMUM OF 4 FEET BEHIND CROSSWALKS (EDGE-TO-EDGE, NOT CENTER-TO-CENTER), PERPENDICULAR TO FACE OF CURB, UNLESS OTHERWISE NOTED.
3. SIGN SUPPORTS INSTALLED IN CONCRETE OR BRICK SHALL BE INSTALLED ON SQUARE PERFORATED TUBULAR STEEL POSTS WITH SLEEVED FOUNDATIONS.
4. ALL PAVEMENT MARKINGS LANE WIDTH DIMENSIONS ARE TO CENTER OF MARKING OR EDGE OF CUTTER (UNLESS OTHERWISE NOTED).
5. REFER TO MUTCD FIGURE 8B-7 FOR THE RAILROAD GRADE CROSSING PAVEMENT MARKING SYMBOL (LEGEND E)



MATCH LINE - STA. 104+00, SEE DWG NO. SN-2.01

F.A.P. NO. AC-STBG-3241(5)E
SHA NO. BC410024
BALTIMORE CITY NO. TR17302

SN-2.02

CITY OF BALTIMORE
DEPARTMENT OF TRANSPORTATION
CONTRACT NO. TR17302

SOUTH CLINTON STREET REHABILITATION
FROM BOSTON STREET TO KEITH AVENUE

SIGNING AND MARKING PLAN

SCALE: N.T.S.

DATE: DECEMBER 2018
SHEET 02 OF 13

SIGNING LEGEND

SYMBOL	DESCRIPTION
	EXISTING GROUND MOUNTED SIGN AND SUPPORT(S)
	PROPOSED GROUND MOUNTED SIGN AND SUPPORT(S)
	EXISTING SIGN TO REMAIN
	EXISTING SIGN TO BE REMOVED
	PROPOSED SIGN TO BE INSTALLED

ADDENDUM NO. 1, PAGE 59 OF 73
347R

DRAWN BY _____
CHECKED BY _____



NO.	REVISIONS	DATE	BY



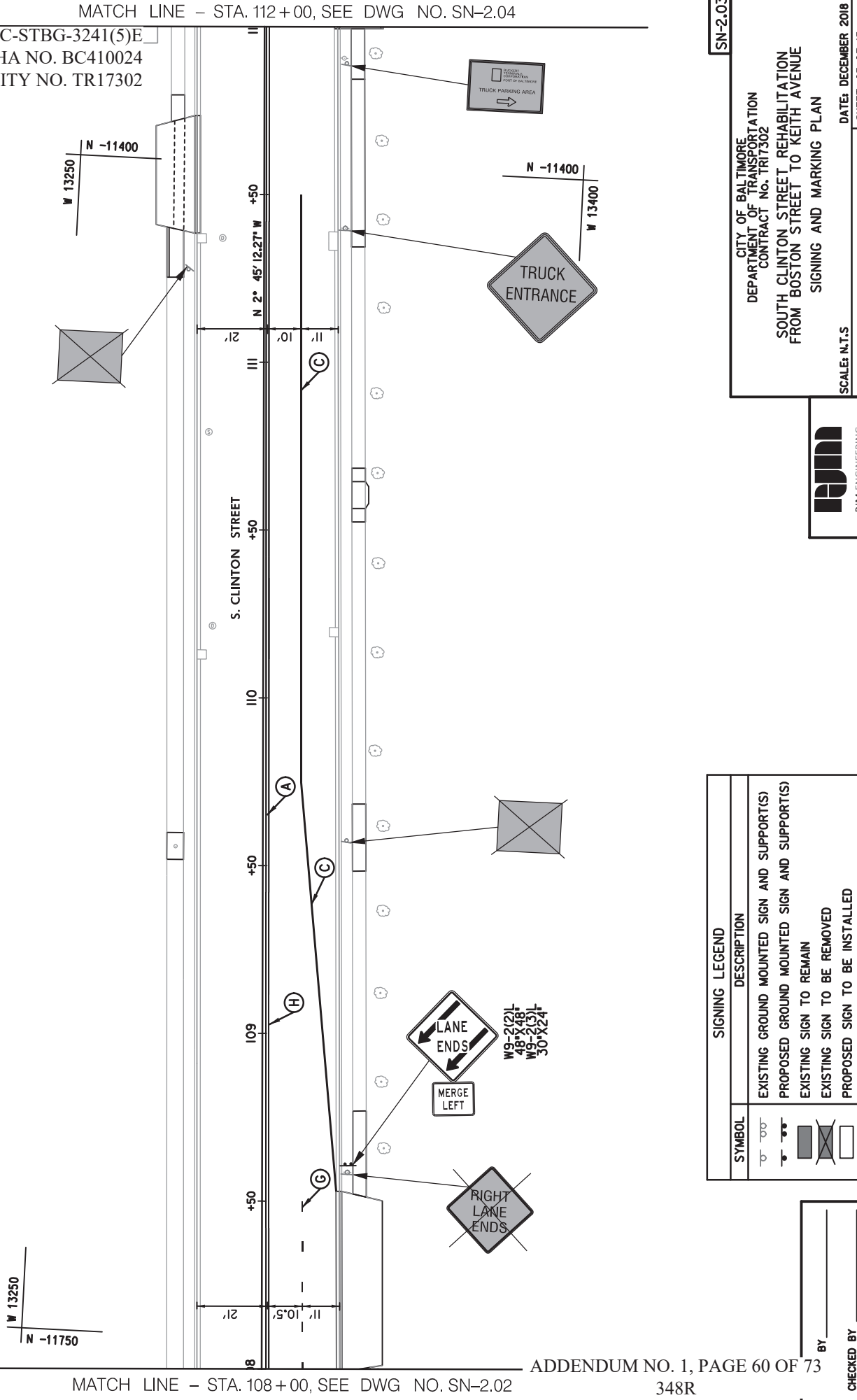
F.A.P. NO. AC-STBG-3241(5)E
 SHA NO. BC410024
 BALTIMORE CITY NO. TR17302

PAVEMENT MARKING LEGEND

- (A) 5 INCH SOLID YELLOW CONTRAST PAVEMENT MARKING TAPE
- (B) 5 INCH WHITE CONTRAST PAVEMENT MARKING TAPE (10' STRIPE, 30' GAP)
- (C) 5 INCH SOLID WHITE CONTRAST PAVEMENT MARKING TAPE
- (D) 24 INCH SOLID WHITE PREFORMED THERMOPLASTIC PAVEMENT MARKING
- (E) RAILROAD GRADE CROSSING PAVEMENT MARKING SYMBOL
- (F) WHITE PREFORMED THERMOPLASTIC PAVEMENT MARKING LEGENDS AND SYMBOLS
- (G) 5 INCH WHITE CONTRAST PAVEMENT MARKING TAPE (3' STRIPE, 10' GAP)
- (H) REMOVE EXISTING PAVEMENT MARKING

SIGNING AND PAVEMENT MARKING GENERAL NOTES

- 1. THE CONTRACTOR SHALL CENTER THE PROPOSED CROSSWALKS ON THE RAMPS.
- 2. THE CONTRACTOR SHALL INSTALL STOPLINES A MINIMUM OF 4 FEET BEHIND CROSSWALKS (EDGE-TO-EDGE, NOT CENTER-TO-CENTER), PERPENDICULAR TO FACE OF CURB, UNLESS OTHERWISE NOTED.
- 3. SIGN SUPPORTS INSTALLED IN CONCRETE OR BRICK SHALL BE INSTALLED ON SQUARE PERFORATED TUBULAR STEEL POSTS WITH SLEEVED FOUNDATIONS.
- 4. ALL PAVEMENT MARKINGS LANE WIDTH DIMENSIONS ARE TO CENTER OF MARKING OR EDGE OF GUTTER (UNLESS OTHERWISE NOTED).



MATCH LINE - STA. 108 +00, SEE DWG NO. SN-2.02

MATCH LINE - STA. 112 +00, SEE DWG NO. SN-2.04

ADDENDUM NO. 1, PAGE 60 OF 73
 348R

CITY OF BALTIMORE
 DEPARTMENT OF TRANSPORTATION
 CONTRACT NO. TR17302

SOUTH CLINTON STREET REHABILITATION
 FROM BOSTON STREET TO KEITH AVENUE

SIGNING AND MARKING PLAN

SCALE: N.T.S. DATE: DECEMBER 2018 SHEET 03 OF 13

SYMBOL	DESCRIPTION
	EXISTING GROUND MOUNTED SIGN AND SUPPORT(S)
	PROPOSED GROUND MOUNTED SIGN AND SUPPORT(S)
	EXISTING SIGN TO REMAIN
	EXISTING SIGN TO BE REMOVED
	PROPOSED SIGN TO BE INSTALLED

PAVEMENT MARKING LEGEND

SYMBOL	DESCRIPTION
	EXISTING GROUND MOUNTED SIGN AND SUPPORT(S)
	PROPOSED GROUND MOUNTED SIGN AND SUPPORT(S)
	EXISTING SIGN TO REMAIN
	EXISTING SIGN TO BE REMOVED
	PROPOSED SIGN TO BE INSTALLED

BY: _____ CHECKED BY: _____

THURSDAY, AUGUST 08, 2019 AT 07:07 PM
 P:\baltimore\clinton Street\0 Design\SN-2.03.Clinton-St.dgn

NO.	REVISIONS	DATE	BY

SIGNING AND PAVEMENT MARKING GENERAL NOTES

1. THE CONTRACTOR SHALL CENTER THE PROPOSED CROSSWALKS ON THE RAMPS.
2. THE CONTRACTOR SHALL INSTALL STOPLINES A MINIMUM OF 4 FEET BEHIND CROSSWALKS (EDGE-TO-EDGE, NOT CENTER-TO-CENTER), PERPENDICULAR TO FACE OF CURB, UNLESS OTHERWISE NOTED.
3. SIGN SUPPORTS INSTALLED IN CONCRETE OR BRICK SHALL BE INSTALLED ON SQUARE PERFORATED TUBULAR STEEL POSTS WITH SLEEVED FOUNDATIONS.
4. ALL PAVEMENT MARKINGS LANE WIDTH DIMENSIONS ARE TO CENTER OF MARKING OR EDGE OF GUTTER (UNLESS OTHERWISE NOTED).

MATCH LINE - STA. 116+00, SEE DWG NO. SN-2.05

F.A.P. NO. AC-STBG-3241(5)E
SHA NO. BC410024
BALTIMORE CITY NO. TR17302

SN-2.04

CITY OF BALTIMORE
DEPARTMENT OF TRANSPORTATION
CONTRACT NO. TR17302
SOUTH CLINTON STREET REHABILITATION
FROM BOSTON STREET TO KEITH AVENUE
SIGNING AND MARKING PLAN

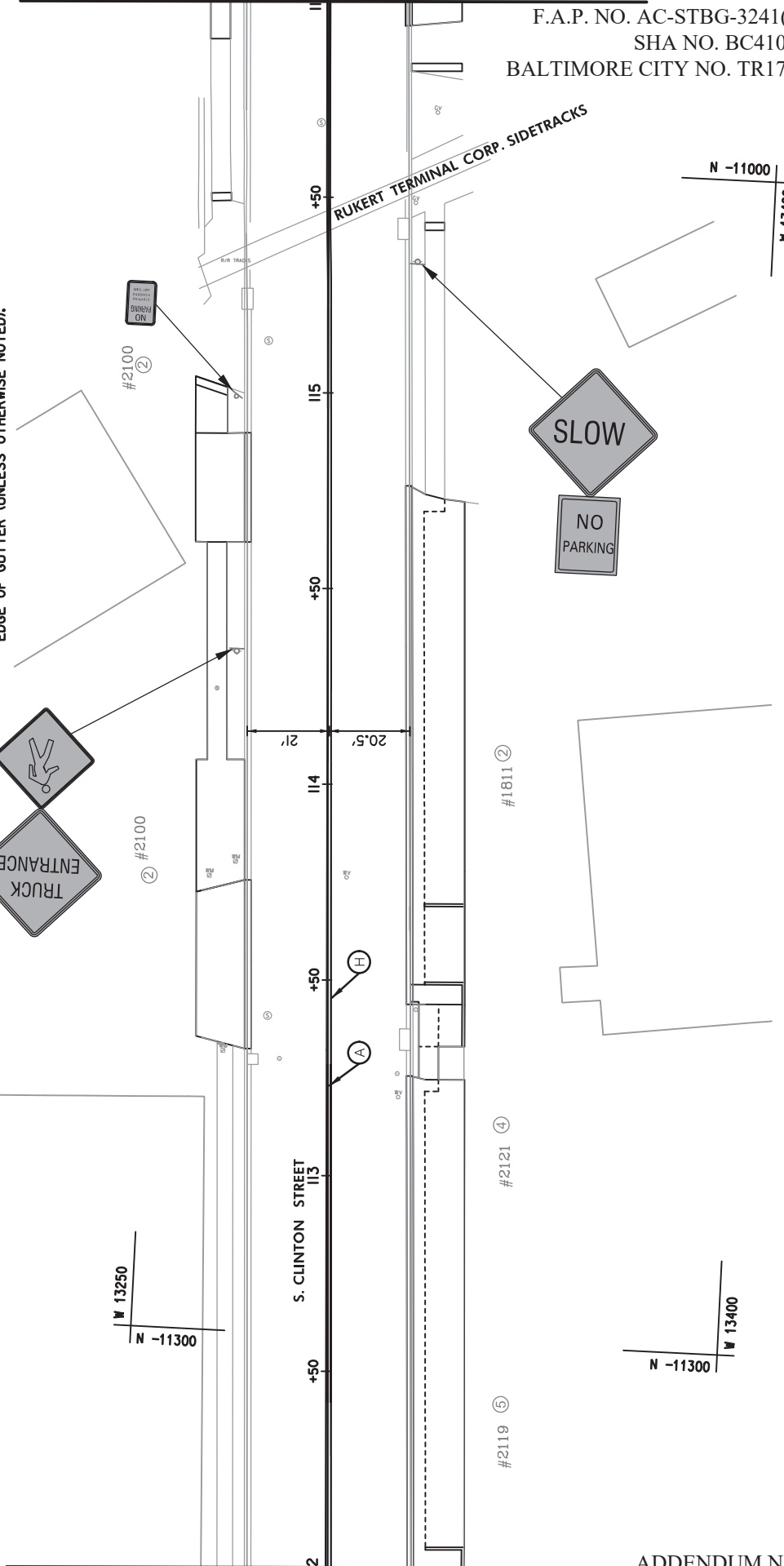
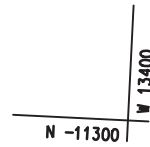
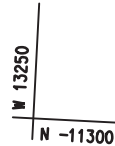
SCALE: N.T.S

DATE: DECEMBER 2018
SHEET 04 OF 13



PAVEMENT MARKING LEGEND

- ④ 5 INCH SOLID DOUBLE YELLOW CONTRAST PAVEMENT MARKING TAPE
- ③ 5 INCH WHITE CONTRAST PAVEMENT MARKING TAPE (10' STRIPE, 30' GAP)
- ② 5 INCH SOLID WHITE CONTRAST PAVEMENT MARKING TAPE
- ① 24 INCH SOLID WHITE PREFORMED THERMOPLASTIC PAVEMENT MARKING
- ⓐ RAILROAD GRADE CROSSING THERMOPLASTIC PAVEMENT MARKING SYMBOL
- ⓑ WHITE PREFORMED THERMOPLASTIC PAVEMENT MARKING LEGENDS AND SYMBOLS
- ⓒ 5 INCH WHITE CONTRAST PAVEMENT MARKING TAPE (3' STRIPE, 10' GAP)
- ⓓ REMOVE EXISTING PAVEMENT MARKING



MATCH LINE - STA. 112+00, SEE DWG NO. SN-2.03

ADDENDUM NO. 1, PAGE 61 OF 73
349R

SIGNING LEGEND	
SYMBOL	DESCRIPTION
	EXISTING GROUND MOUNTED SIGN AND SUPPORT(S)
	PROPOSED GROUND MOUNTED SIGN AND SUPPORT(S)
	EXISTING SIGN TO REMAIN
	EXISTING SIGN TO BE REMOVED
	PROPOSED SIGN TO BE INSTALLED

CHECKED BY _____
BY _____

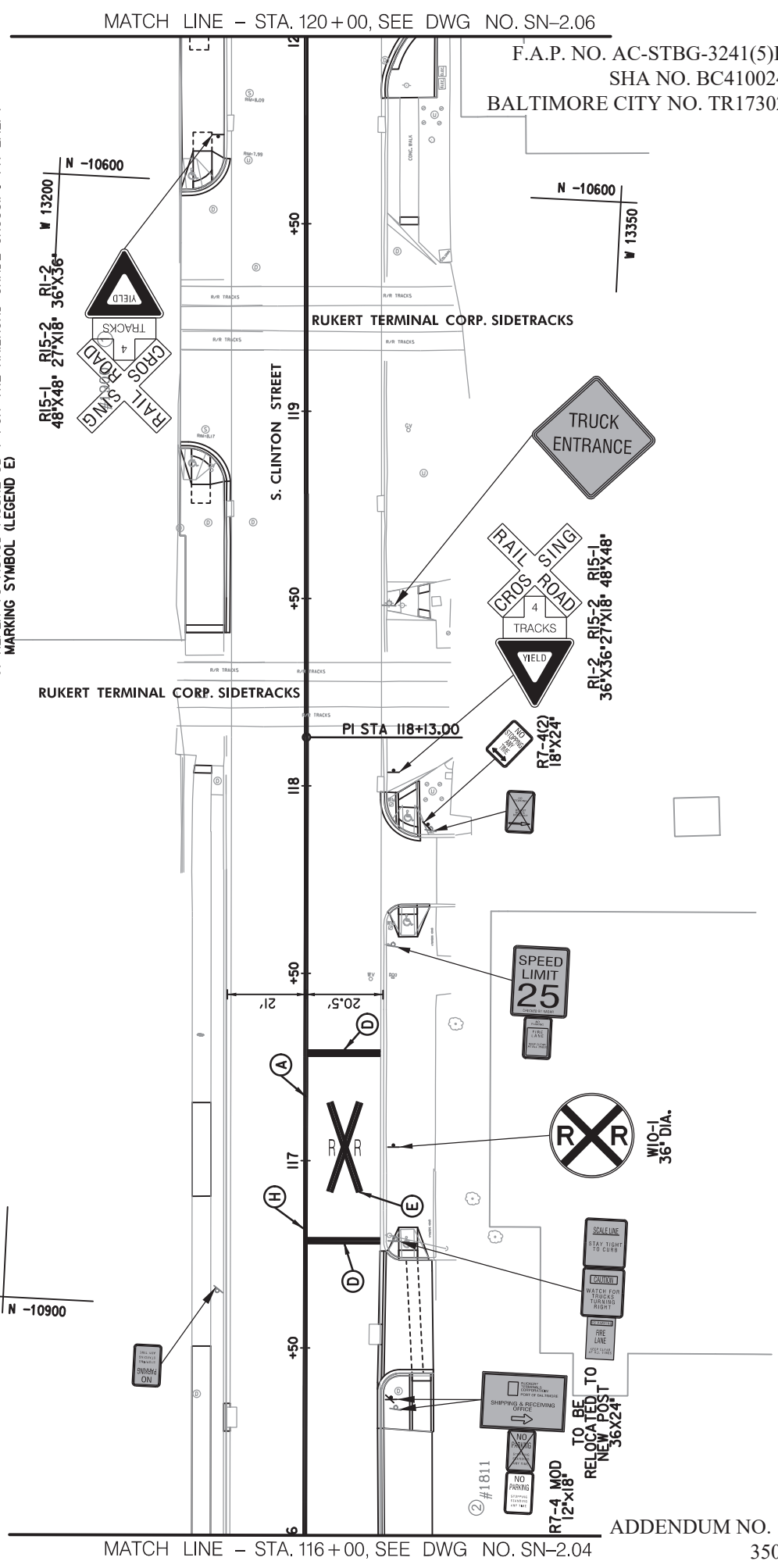
NO.	REVISIONS	DATE	BY

PAVEMENT MARKING LEGEND

- A 5 INCH SOLID DOUBLE YELLOW CONTRAST PAVEMENT MARKING TAPE
- B 5 INCH WHITE CONTRAST PAVEMENT MARKING TAPE (10' STRIPE, 30' GAP)
- C 5 INCH SOLID WHITE CONTRAST PAVEMENT MARKING TAPE
- D 24 INCH SOLID WHITE PREFORMED THERMOPLASTIC PAVEMENT MARKING
- E RAILROAD GRADE CROSSING PAVEMENT MARKING
- F WHITE PREFORMED THERMOPLASTIC PAVEMENT MARKING LEGENDS AND SYMBOLS
- G 5 INCH WHITE CONTRAST PAVEMENT MARKING TAPE (3' STRIPE, 10' GAP)
- H REMOVE EXISTING PAVEMENT MARKING

SIGNING AND PAVEMENT MARKING GENERAL NOTES

1. THE CONTRACTOR SHALL CENTER THE PROPOSED CROSSWALKS ON THE RAMPS.
2. THE CONTRACTOR SHALL INSTALL STOPLINES A MINIMUM OF 4 FEET BEHIND CROSSWALKS (EDGE-TO-EDGE, NOT CENTER-TO-CENTER), PERPENDICULAR TO FACE OF CURB, UNLESS OTHERWISE NOTED.
3. SIGN SUPPORTS INSTALLED IN CONCRETE OR BRICK SHALL BE INSTALLED ON SQUARE PERFORATED TUBULAR STEEL POSTS WITH SLEEVED FOUNDATIONS.
4. ALL PAVEMENT MARKINGS LANE WIDTH DIMENSIONS ARE TO CENTER OF MARKING OR EDGE OF GUTTER (UNLESS OTHERWISE NOTED).
5. REFER TO MUTCD FIGURE 8B-7 FOR THE RAILROAD GRADE CROSSING PAVEMENT MARKING SYMBOL (LEGEND E)



MATCH LINE - STA. 116+00, SEE DWG NO. SN-2.04

MATCH LINE - STA. 120+00, SEE DWG NO. SN-2.06

F.A.P. NO. AC-STBG-3241(5)E
SHA NO. BC410024
BALTIMORE CITY NO. TR17302

SIGNING LEGEND	
SYMBOL	DESCRIPTION
	EXISTING GROUND MOUNTED SIGN AND SUPPORT(S)
	PROPOSED GROUND MOUNTED SIGN AND SUPPORT(S)
	EXISTING SIGN TO REMAIN
	EXISTING SIGN TO BE REMOVED
	PROPOSED SIGN TO BE INSTALLED

SN-2.05

CITY OF BALTIMORE
DEPARTMENT OF TRANSPORTATION
CONTRACT NO. TR17302

SOUTH CLINTON STREET REHABILITATION
FROM BOSTON STREET TO KEITH AVENUE

SIGNING AND MARKING PLAN

SCALE: N.T.S.

DATE: DECEMBER 2018

SHEET 05 OF 13



CHECKED BY _____
BY _____

F.A.P. NO. AC-STBG-3241(5)E
 SHA NO. BC410024
 BALTIMORE CITY NO. TR17302

SN-2.06

CITY OF BALTIMORE
 DEPARTMENT OF TRANSPORTATION
 CONTRACT NO. TR17302
 SOUTH CLINTON STREET REHABILITATION
 FROM BOSTON STREET TO KEITH AVENUE
 SIGNING AND MARKING PLAN

SCALE: N.T.S



DATE: DECEMBER 2018
 SHEET 06 OF 13

REVISIONS

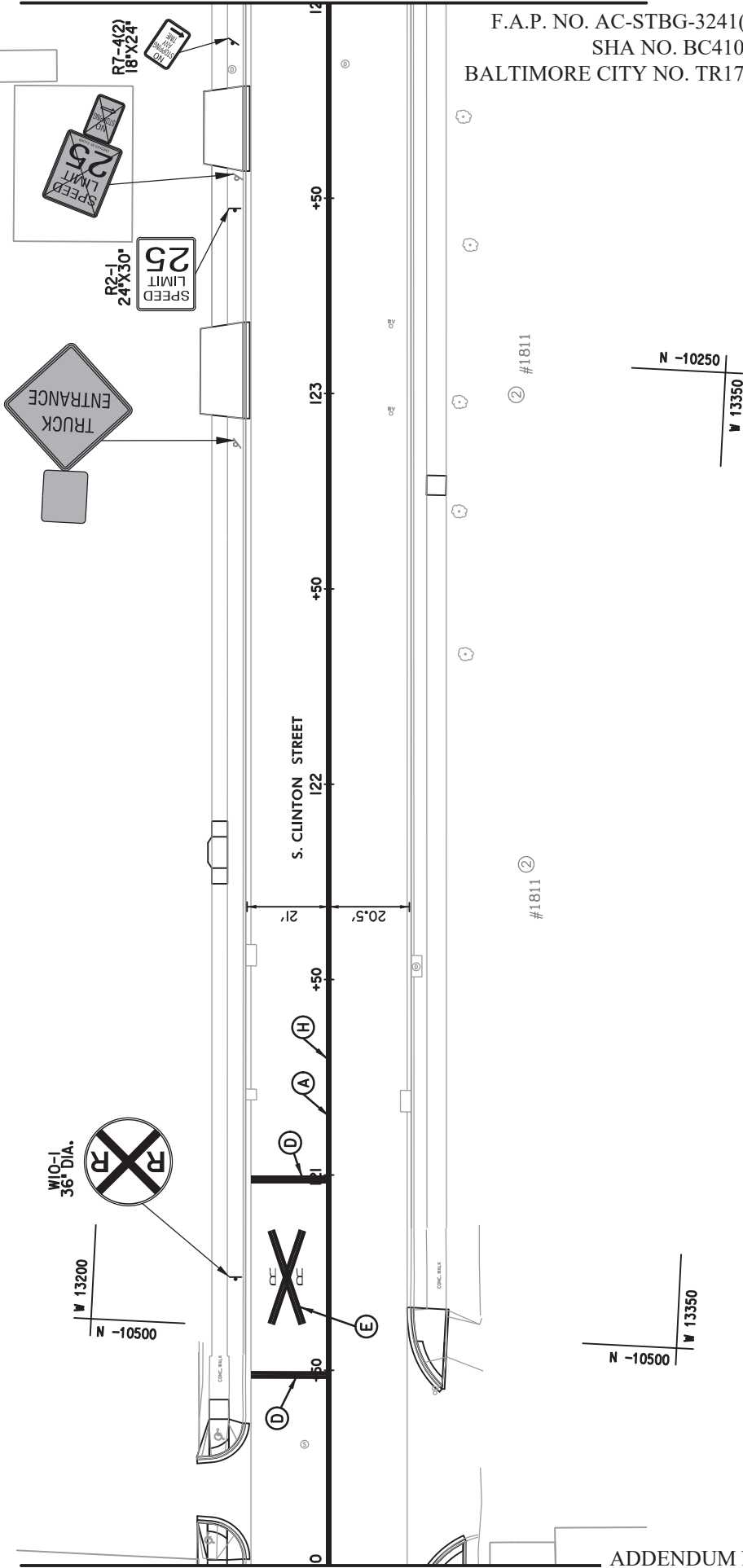
NO.	DESCRIPTION	DATE	BY

PAVEMENT MARKING LEGEND

- (A) 5 INCH SOLID DOUBLE YELLOW CONTRAST PAVEMENT MARKING TAPE
- (B) 5 INCH WHITE CONTRAST PAVEMENT MARKING TAPE (10' STRIPE, 30' GAP)
- (C) 5 INCH SOLID WHITE CONTRAST PAVEMENT MARKING TAPE
- (D) 24 INCH SOLID WHITE CONTRAST THERMOPLASTIC PAVEMENT MARKING
- (E) RAILROAD GRADE CROSSING PAVEMENT MARKING SYMBOL
- (F) WHITE PREFORMED THERMOPLASTIC PAVEMENT MARKING LEGENDS AND SYMBOLS
- (G) 5 INCH WHITE CONTRAST PAVEMENT MARKING TAPE (3' STRIPE, 10' GAP)
- (H) REMOVE EXISTING PAVEMENT MARKING

SIGNING AND PAVEMENT MARKING GENERAL NOTES

1. THE CONTRACTOR SHALL CENTER THE PROPOSED CROSSWALKS ON THE RAMPS.
2. THE CONTRACTOR SHALL INSTALL STOPLINES A MINIMUM OF 4 FEET BEHIND CROSSWALKS (EDGE-TO-EDGE, NOT CENTER-TO-CENTER), PERPENDICULAR TO FACE OF CURB, UNLESS OTHERWISE NOTED.
3. SIGN SUPPORTS INSTALLED IN CONCRETE OR BRICK SHALL BE INSTALLED ON SQUARE PERFORATED TUBULAR STEEL POSTS WITH SLEEVED FOUNDATIONS.
4. ALL PAVEMENT MARKINGS LAINE WIDTH DIMENSIONS ARE TO CENTER OF MARKING OR EDGE OF GUTTER (UNLESS OTHERWISE NOTED).
5. MARKING SYMBOL (LEGEND E) REFER TO MUTCD FIGURE 8B-7 FOR THE RAILROAD GRADE CROSSING PAVEMENT MARKING SYMBOL (LEGEND E)



SIGNING LEGEND	
SYMBOL	DESCRIPTION
	EXISTING GROUND MOUNTED SIGN AND SUPPORT(S)
	PROPOSED GROUND MOUNTED SIGN AND SUPPORT(S)
	EXISTING SIGN TO REMAIN
	EXISTING SIGN TO BE REMOVED
	PROPOSED SIGN TO BE INSTALLED

BY _____
 CHECKED BY _____

F.A.P. NO. AC-STBG-3241(5)E
 SHA NO. BC410024
 BALTIMORE CITY NO. TR17302

SN-2.07

CITY OF BALTIMORE
 DEPARTMENT OF TRANSPORTATION
 CONTRACT NO. TR17302
 SOUTH CLINTON STREET REHABILITATION
 FROM BOSTON STREET TO KEITH AVENUE
 SIGNING AND MARKING PLAN

SCALE: N.T.S.
 DATE: DECEMBER 2018
 SHEET 07 OF 13



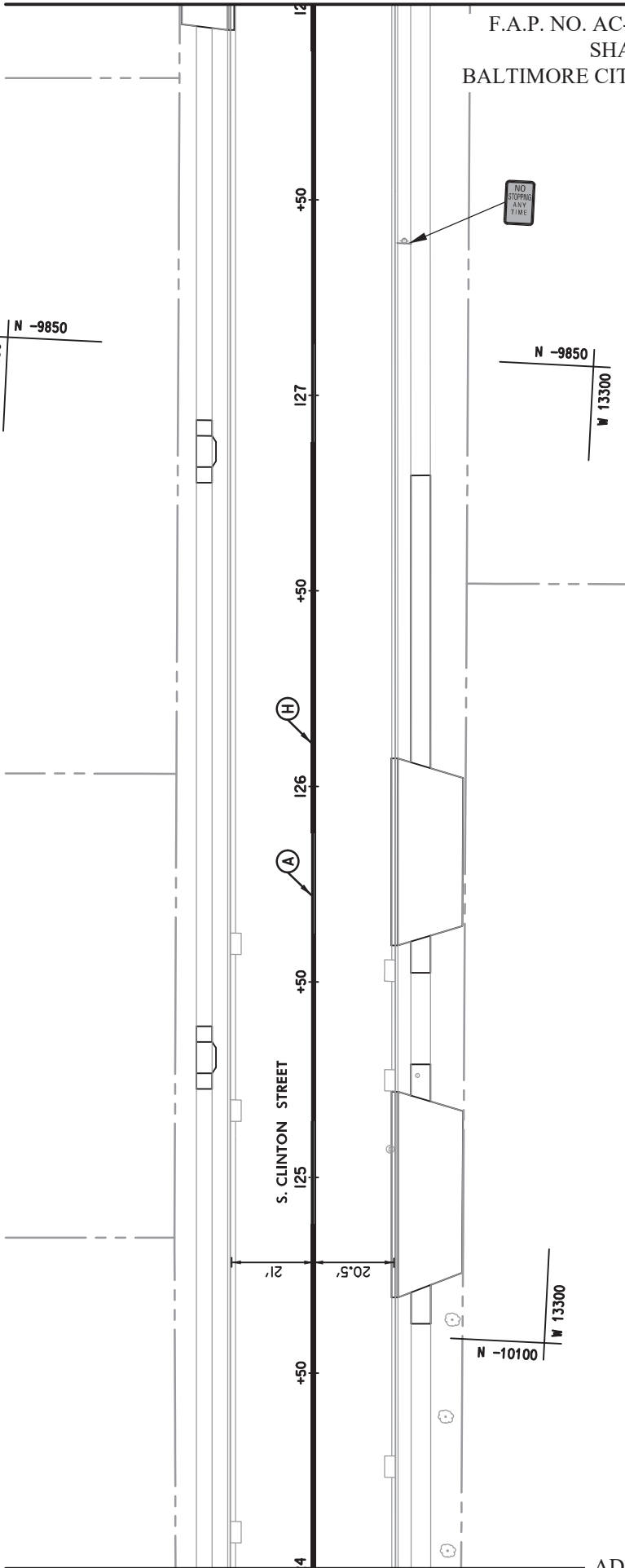
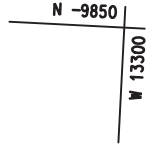
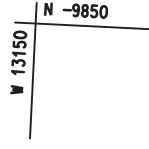
REVISIONS		
NO.	DESCRIPTION	DATE BY

PAVEMENT MARKING LEGEND

- Ⓐ 5 INCH SOLID DOUBLE YELLOW CONTRAST PAVEMENT MARKING TAPE
- Ⓑ 5 INCH WHITE CONTRAST PAVEMENT MARKING TAPE (10' STRIPE, 30' GAP)
- Ⓒ 5 INCH SOLID WHITE CONTRAST PAVEMENT MARKING TAPE
- Ⓓ 24 INCH SOLID WHITE PREFORMED THERMOPLASTIC PAVEMENT MARKING
- Ⓔ RAILROAD GRADE CROSSING PAVEMENT MARKING SYMBOL
- Ⓕ WHITE PREFORMED THERMOPLASTIC PAVEMENT MARKING LEGENDS AND SYMBOLS
- Ⓖ 5 INCH WHITE CONTRAST PAVEMENT MARKING TAPE (3' STRIPE, 10' GAP)
- Ⓗ REMOVE EXISTING PAVEMENT MARKING

SIGNING AND PAVEMENT MARKING GENERAL NOTES

1. THE CONTRACTOR SHALL CENTER THE PROPOSED CROSSWALKS ON THE RAMPS.
2. THE CONTRACTOR SHALL INSTALL STOPLINES A MINIMUM OF 4 FEET BEHIND CROSSWALKS (EDGE-TO-EDGE, NOT CENTER-TO-CENTER), PERPENDICULAR TO FACE OF CURB, UNLESS OTHERWISE NOTED.
3. SIGN SUPPORTS INSTALLED IN CONCRETE OR BRICK SHALL BE INSTALLED ON SQUARE PERFORATED TUBULAR STEEL POSTS WITH SLEEVED FOUNDATIONS.
4. ALL PAVEMENT MARKINGS LAINE WIDTH DIMENSIONS ARE TO CENTER OF MARKING OR EDGE OF GUTTER (UNLESS OTHERWISE NOTED).



SIGNING LEGEND	
SYMBOL	DESCRIPTION
	EXISTING GROUND MOUNTED SIGN AND SUPPORT(S)
	PROPOSED GROUND MOUNTED SIGN AND SUPPORT(S)
	EXISTING SIGN TO REMAIN
	EXISTING SIGN TO BE REMOVED
	PROPOSED SIGN TO BE INSTALLED

BY _____
 CHECKED BY _____

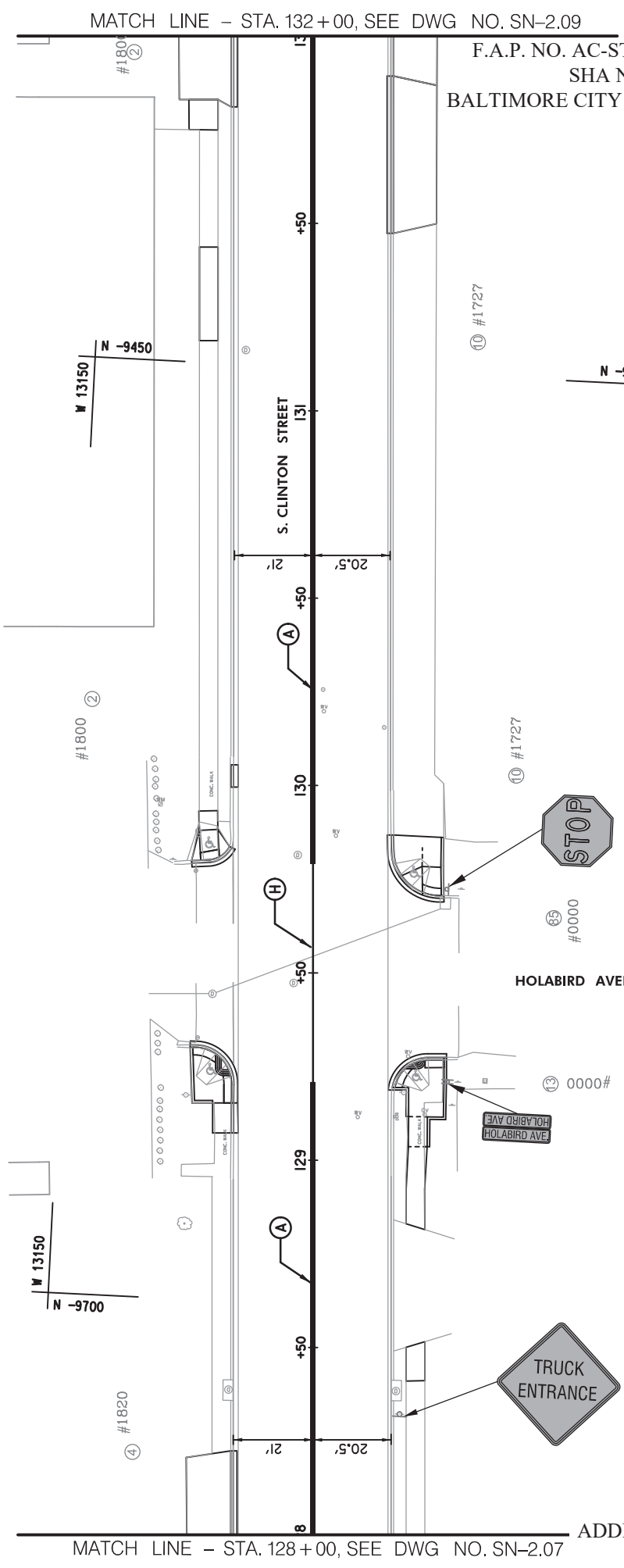
NO.	REVISIONS	DATE	BY

SIGNING AND PAVEMENT MARKING GENERAL NOTES

1. THE CONTRACTOR SHALL CENTER THE PROPOSED CROSSWALKS ON THE RAMPS.
2. THE CONTRACTOR SHALL INSTALL STOPLINES A MINIMUM OF 4 FEET BEHIND CROSSWALKS (EDGE-TO-EDGE, NOT CENTER-TO-CENTER), PERPENDICULAR TO FACE OF CURB, UNLESS OTHERWISE NOTED.
3. SIGN SUPPORTS INSTALLED IN CONCRETE OR BRICK SHALL BE INSTALLED ON SQUARE PERFORATED TUBULAR STEEL POSTS WITH SLEEVED FOUNDATIONS.
4. ALL PAVEMENT MARKINGS LANE WIDTH DIMENSIONS ARE TO CENTER OF MARKING OR EDGE OF GUTTER (UNLESS OTHERWISE NOTED).

PAVEMENT MARKING LEGEND

- A 5 INCH SOLID DOUBLE YELLOW CONTRAST PAVEMENT MARKING TAPE
- B 5 INCH WHITE CONTRAST PAVEMENT MARKING TAPE (10' STRIPE, 30' GAP)
- C 5 INCH SOLID WHITE CONTRAST PAVEMENT MARKING TAPE
- D 24 INCH SOLID WHITE PERFORMED THERMOPLASTIC PAVEMENT MARKING
- E RAILROAD GRADE CROSSING PAVEMENT MARKING SYMBOL
- F WHITE PERFORMED THERMOPLASTIC PAVEMENT MARKING LEGENDS AND SYMBOLS
- G 5 INCH WHITE CONTRAST PAVEMENT MARKING TAPE (3' STRIPE, 10' GAP)
- H REMOVE EXISTING PAVEMENT MARKING



MATCH LINE - STA. 128+00, SEE DWG NO. SN-2.07

MATCH LINE - STA. 132+00, SEE DWG NO. SN-2.09

F.A.P. NO. AC-STBG-3241(5)E
SHA NO. BC410024
BALTIMORE CITY NO. TR17302

SIGNING LEGEND	
SYMBOL	DESCRIPTION
	EXISTING GROUND MOUNTED SIGN AND SUPPORT(S)
	PROPOSED GROUND MOUNTED SIGN AND SUPPORT(S)
	EXISTING SIGN TO REMAIN
	EXISTING SIGN TO BE REMOVED
	PROPOSED SIGN TO BE INSTALLED

SN-2.08

CITY OF BALTIMORE
DEPARTMENT OF TRANSPORTATION
CONTRACT NO. TR17302

SOUTH CLINTON STREET REHABILITATION
FROM BOSTON STREET TO KEITH AVENUE

SIGNING AND MARKING PLAN

SCALE: N.T.S.

DATE: DECEMBER 2018
SHEET 08 OF 13



CHECKED BY _____

BY _____

P:\balto\city\clinton Street\0 Design\SN-2.08.Clinton-St.dgn
Thursday, August 08, 2019 AT 07:07 PM

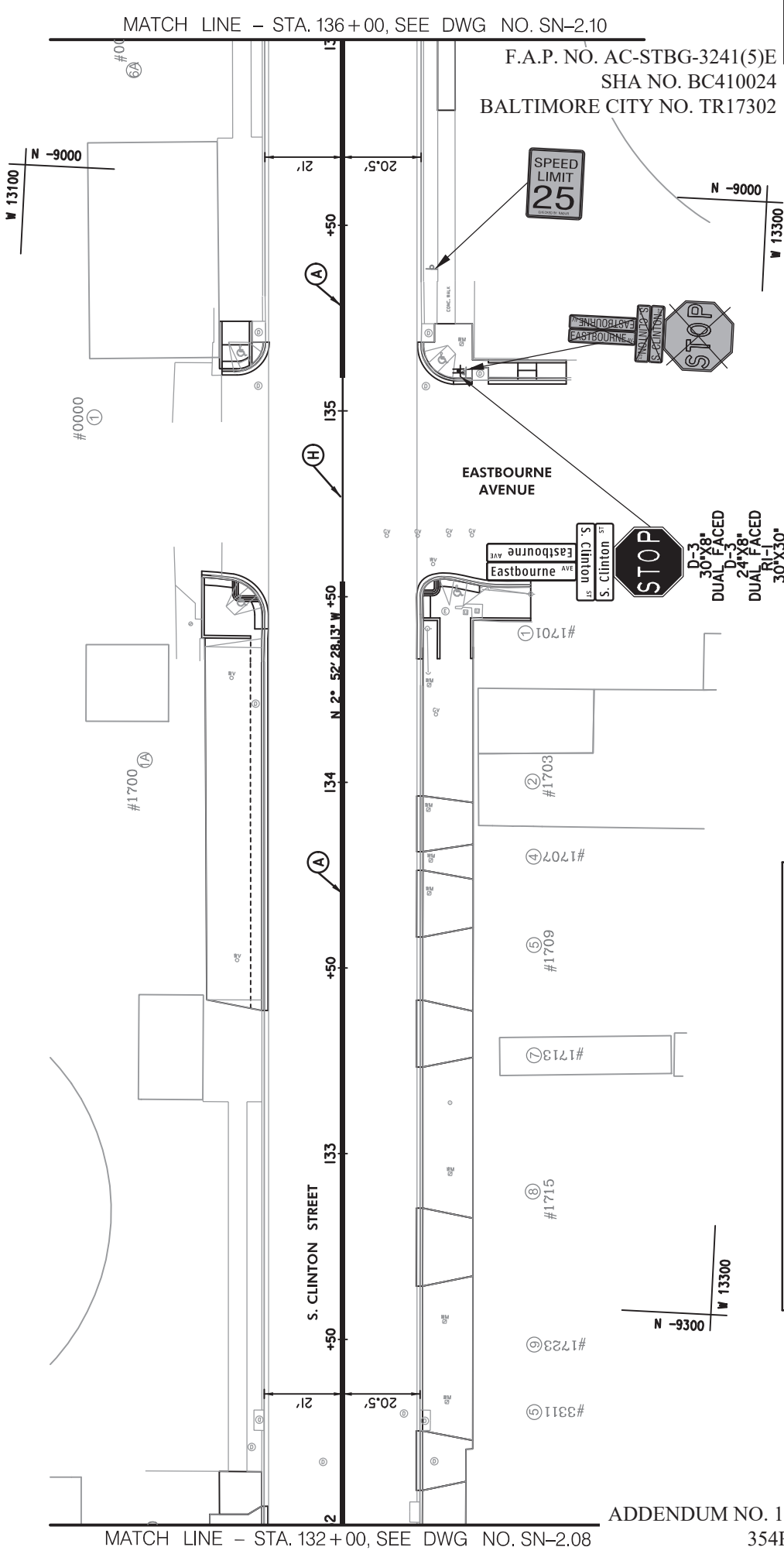
NO.	DESCRIPTION	DATE	BY

SIGNING AND PAVEMENT MARKING GENERAL NOTES

1. THE CONTRACTOR SHALL CENTER THE PROPOSED CROSSWALKS ON THE RAMPS.
2. THE CONTRACTOR SHALL INSTALL STOPLINES A MINIMUM OF 4 FEET BEHIND CROSSWALKS (EDGE-TO-EDGE, NOT CENTER-TO-CENTER), PERPENDICULAR TO FACE OF CURB, UNLESS OTHERWISE NOTED.
3. SIGN SUPPORTS INSTALLED IN CONCRETE OR BRICK SHALL BE INSTALLED ON SQUARE PERFORATED TUBULAR STEEL POSTS WITH SLEEVED FOUNDATIONS.
4. ALL PAVEMENT MARKINGS LANE WIDTH DIMENSIONS ARE TO CENTER OF MARKING OR EDGE OF GUTTER (UNLESS OTHERWISE NOTED).

PAVEMENT MARKING LEGEND

- A 5 INCH SOLID YELLOW CONTRAST PAVEMENT MARKING TAPE
- B 5 INCH WHITE CONTRAST PAVEMENT MARKING TAPE (10' STRIPE, 30' GAP)
- C 5 INCH SOLID WHITE CONTRAST PAVEMENT MARKING TAPE
- D 24 INCH SOLID WHITE PERFORMED THERMOPLASTIC PAVEMENT MARKING
- E RAILROAD GRADE CROSSING PAVEMENT MARKING SYMBOL
- F WHITE PERFORMED THERMOPLASTIC PAVEMENT MARKING LEGENDS AND SYMBOLS
- G 5 INCH WHITE CONTRAST PAVEMENT MARKING TAPE (3' STRIPE, 10' GAP)
- H REMOVE EXISTING PAVEMENT MARKING



SIGNING LEGEND	
SYMBOL	DESCRIPTION
	EXISTING GROUND MOUNTED SIGN AND SUPPORT(S)
	PROPOSED GROUND MOUNTED SIGN AND SUPPORT(S)
	EXISTING SIGN TO REMAIN
	EXISTING SIGN TO BE REMOVED
	PROPOSED SIGN TO BE INSTALLED

SN-2.09

CITY OF BALTIMORE
DEPARTMENT OF TRANSPORTATION
CONTRACT NO. TR17302

**SOUTH CLINTON STREET REHABILITATION
FROM BOSTON STREET TO KEITH AVENUE
SIGNING AND MARKING PLAN**

SCALE: N.T.S.

DATE: DECEMBER 2018
SHEET 09 OF 13



Checked By: _____
By: _____
P:\balto\city\clinton Street\0 Design\PSN-2.09.Clinton-St.dwg
Thursday, August 08, 2019 AT 07:07 PM



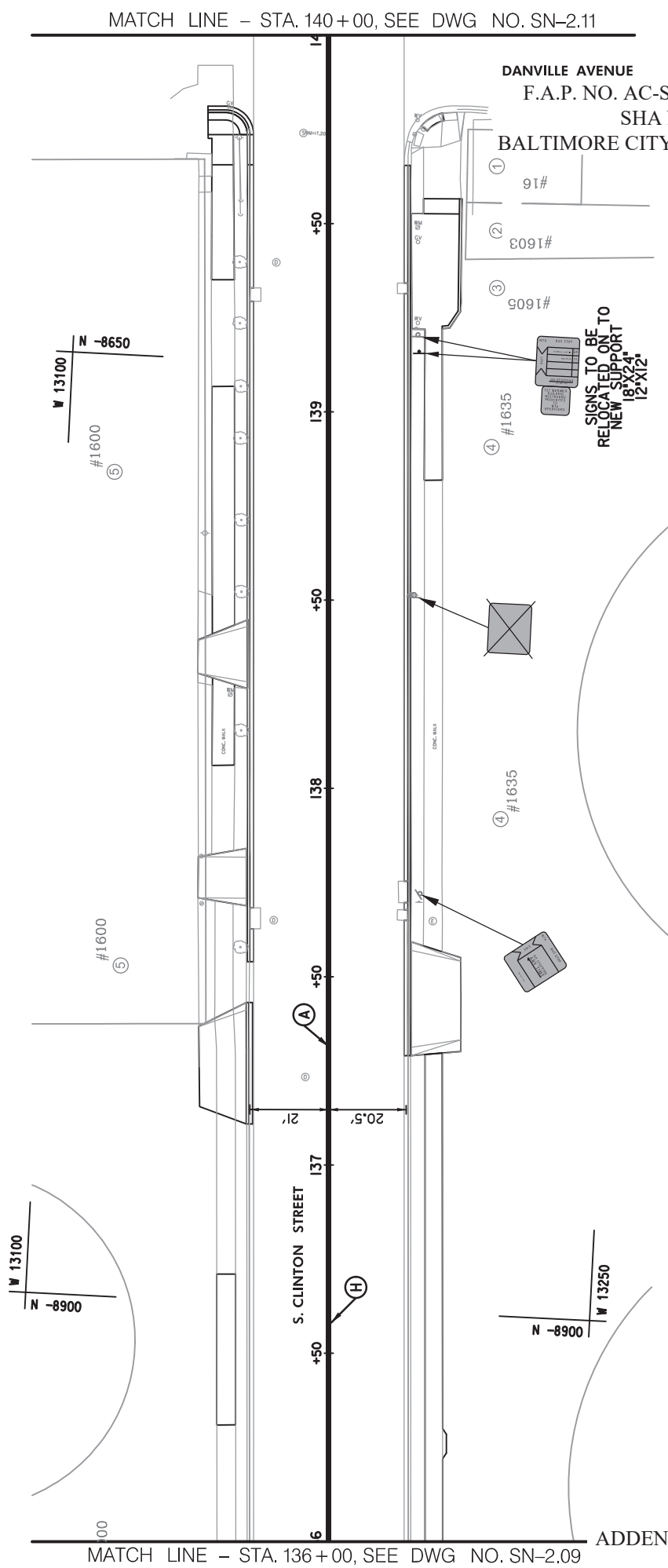
SIGNING AND PAVEMENT MARKING GENERAL NOTES

1. THE CONTRACTOR SHALL CENTER THE PROPOSED CROSSWALKS ON THE RAMPS.
2. THE CONTRACTOR SHALL INSTALL STOPLINES A MINIMUM OF 4 FEET BEHIND CROSSWALKS (EDGE-TO-EDGE, NOT CENTER-TO-CENTER), PERPENDICULAR TO FACE OF CURB, UNLESS OTHERWISE NOTED.
3. SIGN SUPPORTS INSTALLED IN CONCRETE OR BRICK SHALL BE INSTALLED ON SQUARE PERFORATED TUBULAR STEEL POSTS WITH SLEEVED FOUNDATIONS.
4. ALL PAVEMENT MARKINGS LANE WIDTH DIMENSIONS ARE TO CENTER OF MARKING OR EDGE OF GUTTER (UNLESS OTHERWISE NOTED).

PAVEMENT MARKING LEGEND

- A 5 INCH SOLID YELLOW CONTRAST PAVEMENT MARKING TAPE
- B 5 INCH WHITE CONTRAST PAVEMENT MARKING TAPE (10' STRIPE, 30' GAP)
- C 5 INCH SOLID WHITE CONTRAST PAVEMENT MARKING TAPE
- D 24 INCH SOLID WHITE PERFORMED THERMOPLASTIC PAVEMENT MARKING
- E RAILROAD GRADE CROSSING PAVEMENT MARKING SYMBOL
- F WHITE PERFORMED THERMOPLASTIC PAVEMENT MARKING LEGENDS AND SYMBOLS
- G 5 INCH WHITE CONTRAST PAVEMENT MARKING TAPE (3' STRIPE, 10' GAP)
- H REMOVE EXISTING PAVEMENT MARKING

NO.	DESCRIPTION	DATE	BY



MATCH LINE - STA. 136+00, SEE DWG NO. SN-2.09

MATCH LINE - STA. 140+00, SEE DWG NO. SN-2.11

SIGNING LEGEND	
SYMBOL	DESCRIPTION
	EXISTING GROUND MOUNTED SIGN AND SUPPORT(S)
	PROPOSED GROUND MOUNTED SIGN AND SUPPORT(S)
	EXISTING SIGN TO REMAIN
	EXISTING SIGN TO BE REMOVED
	PROPOSED SIGN TO BE INSTALLED

CHECKED BY _____

BY _____

SN-2.10

CITY OF BALTIMORE
DEPARTMENT OF TRANSPORTATION
CONTRACT NO. TR17302

SOUTH CLINTON STREET REHABILITATION
FROM BOSTON STREET TO KEITH AVENUE

SIGNING AND MARKING PLAN

SCALE: N.T.S

DATE: DECEMBER 2018

SHEET 10 OF 13



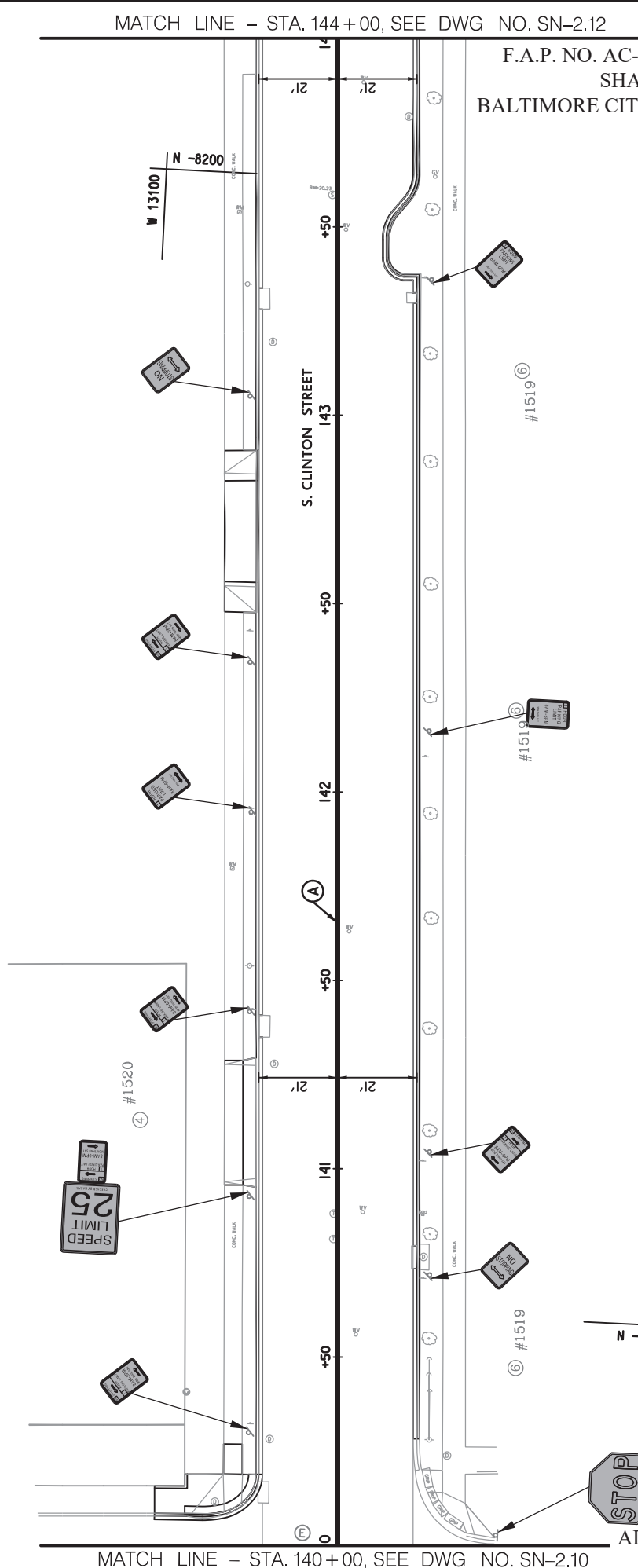
NO.	DESCRIPTION	DATE	BY

SIGNING AND PAVEMENT MARKING GENERAL NOTES

1. THE CONTRACTOR SHALL CENTER THE PROPOSED CROSSWALKS ON THE RAMPS.
2. THE CONTRACTOR SHALL INSTALL STOPLINES A MINIMUM OF 4 FEET BEHIND CROSSWALKS (EDGE-TO-EDGE, NOT CENTER-TO-CENTER), PERPENDICULAR TO FACE OF CURB, UNLESS OTHERWISE NOTED.
3. SIGN SUPPORTS INSTALLED IN CONCRETE OR BRICK SHALL BE INSTALLED ON SQUARE PERFORATED TUBULAR STEEL POSTS WITH SLEEVED FOUNDATIONS.
4. ALL PAVEMENT MARKINGS LANE WIDTH DIMENSIONS ARE TO CENTER OF MARKING OR EDGE OF GUTTER (UNLESS OTHERWISE NOTED).

PAVEMENT MARKING LEGEND

- A 5 INCH SOLID YELLOW CONTRAST PAVEMENT MARKING TAPE
- B 5 INCH WHITE CONTRAST PAVEMENT MARKING TAPE (10' STRIPE, 30' GAP)
- C 5 INCH SOLID WHITE CONTRAST PAVEMENT MARKING TAPE
- D 24 INCH SOLID WHITE PREFORMED THERMOPLASTIC PAVEMENT MARKING
- E RAILROAD GRADE CROSSING PAVEMENT MARKING SYMBOL
- F WHITE PREFORMED THERMOPLASTIC PAVEMENT MARKING LEGENDS AND SYMBOLS
- G 5 INCH WHITE CONTRAST PAVEMENT MARKING TAPE (3' STRIPE, 10' GAP)
- H REMOVE EXISTING PAVEMENT MARKING



MATCH LINE - STA. 140+00, SEE DWG NO. SN-2.10

ADDENDUM NO. 1, PAGE 68 OF 73
356R

F.A.P. NO. AC-STBG-3241(5)E
SHA NO. BC410024
BALTIMORE CITY NO. TR17302

SN-2.11

CITY OF BALTIMORE
DEPARTMENT OF TRANSPORTATION
CONTRACT NO. TR17302

SOUTH CLINTON STREET REHABILITATION
FROM BOSTON STREET TO KEITH AVENUE

SIGNING AND MARKING PLAN

SCALE: N.T.S.

DATE: DECEMBER 2018
SHEET 11 OF 13



SIGNING LEGEND	
SYMBOL	DESCRIPTION
	EXISTING GROUND MOUNTED SIGN AND SUPPORT(S)
	PROPOSED GROUND MOUNTED SIGN AND SUPPORT(S)
	EXISTING SIGN TO REMAIN
	EXISTING SIGN TO BE REMOVED
	PROPOSED SIGN TO BE INSTALLED

BY _____
D BY _____

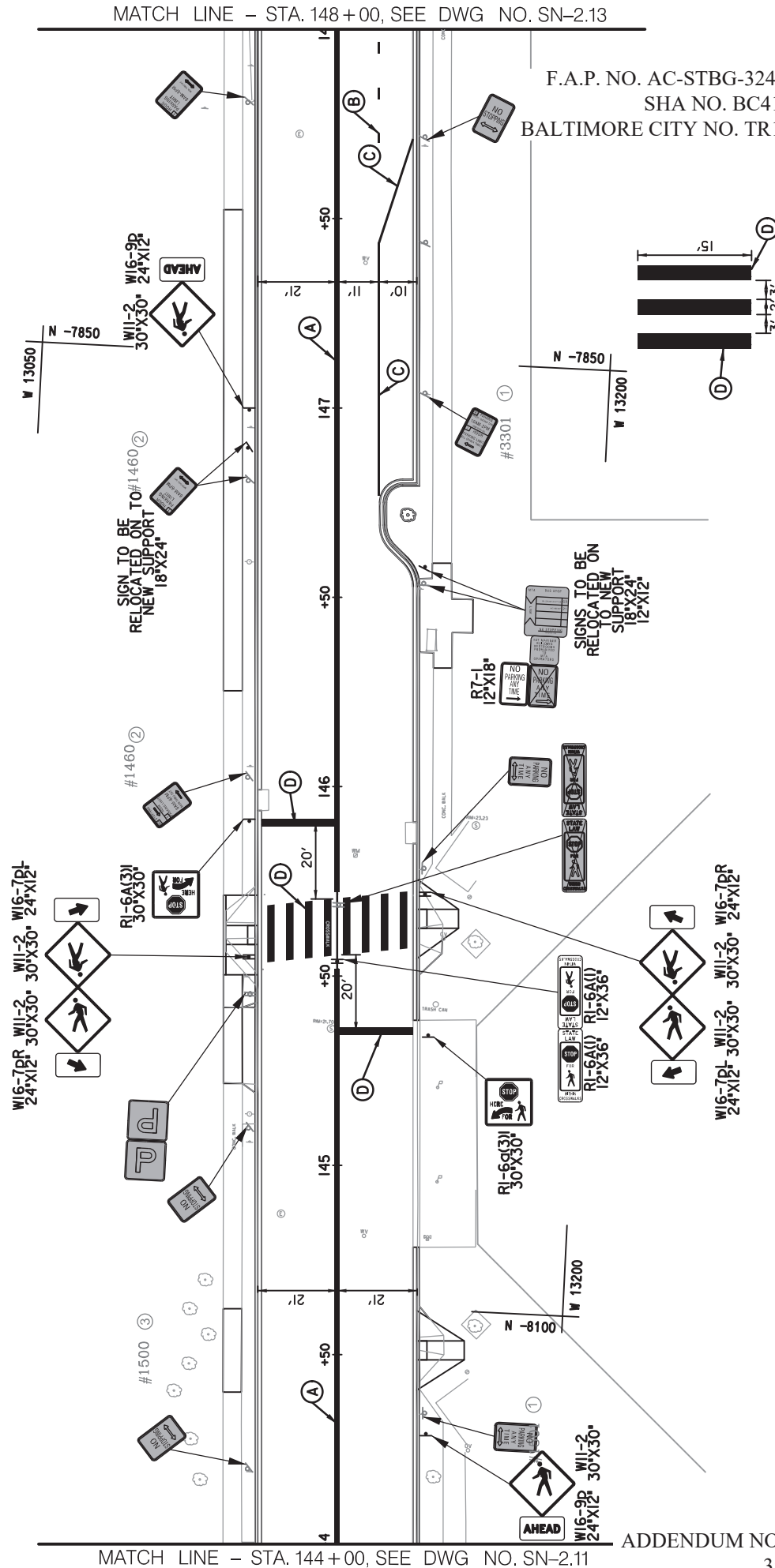
NO.	DESCRIPTION	DATE	BY

SIGNING AND PAVEMENT MARKING GENERAL NOTES

1. THE CONTRACTOR SHALL CENTER THE PROPOSED CROSSWALKS ON THE RAMPS.
2. THE CONTRACTOR SHALL INSTALL STOPLINES A MINIMUM OF 4 FEET BEHIND CROSSWALKS (EDGE-TO-EDGE, NOT CENTER-TO-CENTER), PERPENDICULAR TO FACE OF CURB, UNLESS OTHERWISE NOTED.
3. SIGN SUPPORTS INSTALLED IN CONCRETE OR BRICK SHALL BE INSTALLED ON SQUARE PERFORATED TUBULAR STEEL POSTS WITH SLEEVED FOUNDATIONS.
4. ALL PAVEMENT MARKINGS LANE WIDTH DIMENSIONS ARE TO CENTER OF MARKING OR EDGE OF GUTTER (UNLESS OTHERWISE NOTED).

PAVEMENT MARKING LEGEND

- A 5 INCH SOLID YELLOW CONTRAST PAVEMENT MARKING TAPE
- B 5 INCH WHITE CONTRAST PAVEMENT MARKING TAPE (10' STRIPE, 30' GAP)
- C 5 INCH SOLID WHITE CONTRAST PAVEMENT MARKING TAPE
- D RAILROAD GRADE CROSSING PAVEMENT MARKING SYMBOL
- E WHITE PREFORMED THERMOPLASTIC PAVEMENT MARKING LEGENDS AND SYMBOLS
- F 5 INCH WHITE CONTRAST PAVEMENT MARKING TAPE (3' STRIPE, 10' GAP)
- G REMOVE EXISTING PAVEMENT MARKING



SYMBOL	DESCRIPTION
	EXISTING GROUND MOUNTED SIGN AND SUPPORT(S)
	PROPOSED GROUND MOUNTED SIGN AND SUPPORT(S)
	EXISTING SIGN TO REMAIN
	EXISTING SIGN TO BE REMOVED
	PROPOSED SIGN TO BE INSTALLED

CROSSWALK MARKING DETAIL
 SN-2.12
 CITY OF BALTIMORE
 DEPARTMENT OF TRANSPORTATION
 CONTRACT NO. TR1730Z
 SOUTH CLINTON STREET REHABILITATION
 FROM BOSTON STREET TO KEITH AVENUE
 SIGNING AND MARKING PLAN
 SCALE: N.T.S.
 DATE: DECEMBER 2018
 SHEET 12 OF 13



MATCH LINE - STA. 144 +00, SEE DWG NO. SN-2.11

ADDENDUM NO. 1, PAGE 69 OF 73

357R

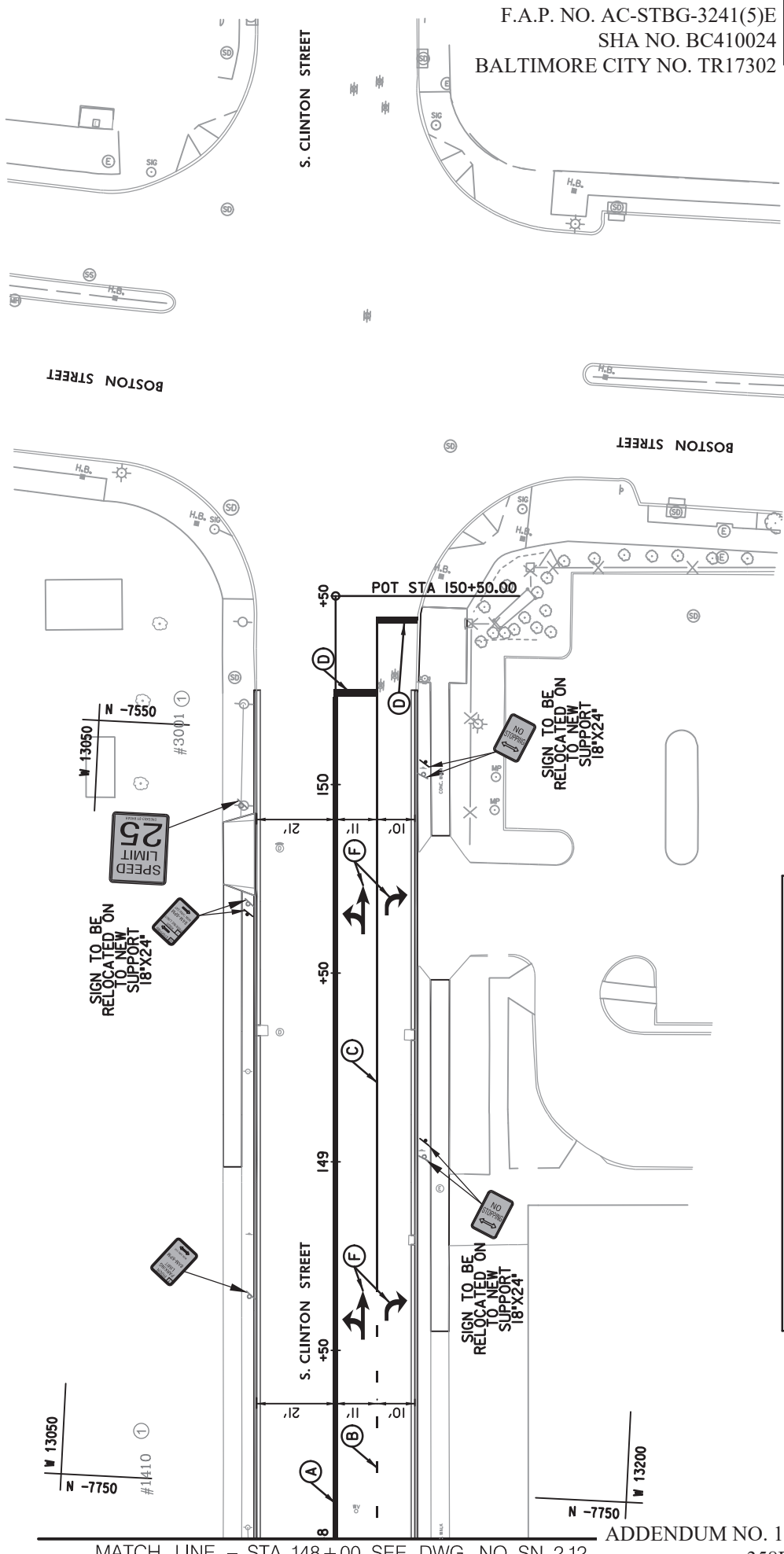
NO.	DESCRIPTION	DATE	BY

SIGNING AND PAVEMENT MARKING GENERAL NOTES

1. THE CONTRACTOR SHALL CENTER THE PROPOSED CROSSWALKS ON THE RAMPS.
2. THE CONTRACTOR SHALL INSTALL STOPLINES A MINIMUM OF 4 FEET BEHIND CROSSWALKS (EDGE-TO-EDGE, NOT CENTER-TO-CENTER), PERPENDICULAR TO FACE OF CURB, UNLESS OTHERWISE NOTED.
3. SIGN SUPPORTS INSTALLED IN CONCRETE OR BRICK SHALL BE INSTALLED ON SQUARE PERFORATED TUBULAR STEEL POSTS WITH SLEEVED FOUNDATIONS.
4. ALL PAVEMENT MARKINGS LANE WIDTH DIMENSIONS ARE TO CENTER OF MARKING OR EDGE OF GUTTER (UNLESS OTHERWISE NOTED).

PAVEMENT MARKING LEGEND

- A 5 INCH SOLID YELLOW CONTRAST PAVEMENT MARKING TAPE
- B 5 INCH WHITE CONTRAST PAVEMENT MARKING TAPE (10' STRIPE, 30' GAP)
- C 5 INCH SOLID WHITE CONTRAST PAVEMENT MARKING TAPE
- D 24 INCH SOLID WHITE PERFORMED THERMOPLASTIC PAVEMENT MARKING
- E RAILROAD GRADE CROSSING PAVEMENT MARKING SYMBOL
- F WHITE PERFORMED THERMOPLASTIC PAVEMENT MARKING LEGENDS AND SYMBOLS
- G 5 INCH WHITE CONTRAST PAVEMENT MARKING TAPE (3' STRIPE, 10' GAP)
- H REMOVE EXISTING PAVEMENT MARKING



MATCH LINE - STA. 148 + 00, SEE DWG NO. SN-2.12

ADDENDUM NO. 1, PAGE 70 OF 73
358R

SYMBOL	DESCRIPTION
	EXISTING GROUND MOUNTED SIGN AND SUPPORT(S)
	PROPOSED GROUND MOUNTED SIGN AND SUPPORT(S)
	EXISTING SIGN TO REMAIN
	EXISTING SIGN TO BE REMOVED
	PROPOSED SIGN TO BE INSTALLED

CHECKED BY _____
BY _____

F.A.P. NO. AC-STBG-3241(5)E
SHA NO. BC410024
BALTIMORE CITY NO. TR17302

SN-2.13

CITY OF BALTIMORE
DEPARTMENT OF TRANSPORTATION
CONTRACT NO. TR17302
SOUTH CLINTON STREET REHABILITATION
FROM BOSTON STREET TO KEITH AVENUE
SIGNING AND MARKING PLAN



SCALE: N.T.S.

DATE: DECEMBER 2018
SHEET 13 OF 13

SCHEDULE OF PRICES

NOTE: THIS PROPOSAL SHALL BE FILLED IN BY THE BIDDER, WITH THE PRICES WRITTEN IN WORDS AND NUMERALS. EXTENSIONS SHALL BE COMPLETED BY THE BIDDER. FOR COMPLETE INFORMATION CONCERNING THESE ITEMS, SEE THE SPECIFICATIONS, SPECIAL PROVISIONS AND CONTRACT FORM.

ITEM NOS.	APPROX. QUANTITIES	DESCRIPTION OF ITEMS AND PRICES BID (IN WRITTEN WORDS)	UNIT PRICE		AMOUNT	
			DOLLARS	CENTS	DOLLARS	CENTS
201	2,170	CCN 200010 CUBIC YARDS OF --- Class 1 Excavation AT _____ PER CUBIC YARD				
202	700	CCN 200020 CUBIC YARDS OF --- Class 1A Excavation AT _____ PER CUBIC YARD				
203	120	CCN 200130 CUBIC YARDS OF --- Class 2 Excavation AT _____ PER CUBIC YARD				
204	50	CCN 200180 CUBIC YARDS OF --- Test Pit Excavation AT _____ PER CUBIC YARD				
205	72	CCN 200290 LINEAR FEET OF --- Removal of Combination Curb & Gutter AT _____ PER LINEAR FOOT				
		END OF CATEGORY NO. 2				

F.A.P. NO. AC-STBG-3241(5)E
S.H.A. NO. BC410024
BALTIMORE CITY NO. TR17302

SCHEDULE OF PRICES

NOTE: THIS PROPOSAL SHALL BE FILLED IN BY THE BIDDER, WITH THE PRICES WRITTEN IN WORDS AND NUMERALS. EXTENSIONS SHALL BE COMPLETED BY THE BIDDER. FOR COMPLETE INFORMATION CONCERNING THESE ITEMS, SEE THE SPECIFICATIONS, SPECIAL PROVISIONS AND CONTRACT FORM.

ITEM NOS.	APPROX. QUANTITIES	DESCRIPTION OF ITEMS AND PRICES BID (IN WRITTEN WORDS)	UNIT PRICE		AMOUNT	
			DOLLARS	CENTS	DOLLARS	CENTS
519	835	LINEAR FEET OF --- 24 Inch White Preformed Thermoplastic Pavement Markings AT _____ PER LINEAR FOOT				
520	825	SQUARE FEET OF --- White Preformed Thermoplastic Pavement Marking Legends And Symbols AT _____ PER SQUARE FOOT				
521	3,000	LINEAR FEET OF --- Cleaning And Sealing Of Cracks In Existing Concrete Pavement AT _____ PER LINEAR FOOT				
522	3,000	LINEAR FEET OF --- Cleaning And Sealing Of Existing Concrete Pavement Joints AT _____ PER LINEAR FOOT				
		END OF CATEGORY NO. 5				

F.A.P. NO. AC-STBG-3241(5)E
S.H.A. NO. BC410024
BALTIMORE CITY NO. TR17302

SCHEDULE OF PRICES

NOTE: THIS PROPOSAL SHALL BE FILLED IN BY THE BIDDER, WITH THE PRICES WRITTEN IN WORDS AND NUMERALS. EXTENSIONS SHALL BE COMPLETED BY THE BIDDER. FOR COMPLETE INFORMATION CONCERNING THESE ITEMS, SEE THE SPECIFICATIONS, SPECIAL PROVISIONS AND CONTRACT FORM.

	TOTAL BID (IN WRITTEN WORDS) <u>USING THE FOLLOWING ITEMS</u>	AMOUNT					
		DOLLARS	CENTS				
	101-130, 201-205, 301-313, 501-522, 601-606, 701-706, 801-806						
	TOTAL BID						

F.A.P. NO. AC-STBG-3241 (5)E
 S.H.A. NO. BC410024
 BALTIMORE CITY NO. TR17302